

Social, Economic and Environmental Impact Assessment for the Chapais and Chibougamau Region

Construction of a Logging Road for Oversized Trucks

Answers to supplementary questions

Presented by:

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December 2009



Environnement
industriel **Bugnon inc.**

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5 Introduction

Upon receipt of the supplementary questions brought up by the Review Committee (REVCOM) following the submittal of the document entitled *Étude d'impact social, économique et sur l'environnement de la région de Chapais et Chibougamau : Construction d'un chemin forestier de transport de bois pour camion hors norme* (Social, Economic and Environmental Impact Assessment for the Chapais and Chibougamau Région: Construction of a Logging Road for Oversized Trucks), a series of actions have been taken in order to respond appropriately to all the questions.

Several private consultations were held. Consultations were held with tallymen affected by the project, officials of the MDDEP – Direction du patrimoine écologique, archaeologists and representatives of the MRNF.

The questions have been grouped according to the issues raised by the REVCOM in the supplementary questions. Hence, the following issues are dealt with:

1. Project rationale
2. Assinica Park
3. General information
4. Woodland caribou
5. Private consultations
6. Bridges and watercourse crossings
7. Archaeological considerations
8. Forest camp

Accordingly, the following paragraphs provide answers to the supplementary questions regarding the assessment, with the objective of clarifying the document submitted in July of last year. Specific answers to the questions are highlighted in bold in such paragraphs, in order to make it easier and faster to locate them within the body of the text.

6 Project rationale

- 6.1 Question 1: *The project proponent will explain and corroborate the various data presented regarding project costs and the alternatives that have been studied.*

At the time of drafting of the first environmental assessment document, there was some confusion with respect to the designation of the various categories of Cree land. As a matter of fact, wherever “Category I land” is mentioned, the text should read “Category III land”.

This mix-up was not noticed by any reviewer and the editorial team apologizes for it.

With regards to road construction costs, the usual cost of transportation is estimated at around \$0.09 per m³ per kilometre of transportation. Thus, given that more than 2.8 million m³ of timber will be harvested over time and that the northern passage will increase by 100 km the distance over which timber is transported, the difference between the planned corridor and the northern passage is estimated at \$26.11 million. Table 6.1 shows the detailed calculation. Road maintenance costs must be added to the total.

Table 6.1: Detailed cost of timber transportation for two options: transport via the planned extension of road L-209-N or via the northern route.

Detailed costs	Northern passage	Extension of road L-209-N
Timber transportation cost	\$0.09/m ³ /km	\$0.09/m ³ /km
Transportation distance (km)	196	96
Volume of timber to be harvested	2.8 x 10 ⁶ m ³	2.8 x 10 ⁶ m ³
Road maintenance costs	\$65,000/yr	\$132,000/yr
Maintenance costs over 15 years	\$1,990,000	\$975,000
Total	\$51,382,625	\$25,267,800

Considering that the construction of the road will cost \$12.5 million, Barrette-Chapais Itée expects to save \$13.6 million.

7 Assinica Park

- 7.1 Question 2: In the area of Km 53-54, does the logging road corridor actually cross the future Assinica Cree Heritage Park or is this a mapping error? If so, what is the opinion of the ministère du Développement durable, de l'Environnement et des Parcs (MDDEP) – Direction du patrimoine écologique et des parcs regarding this encroachment on the future Park?

As explained below in the section regarding private consultations, the final boundaries of the future Assinica Park are still being negotiated. When the first draft of this impact assessment was being prepared, the boundaries of the future park had not yet been finalized. Consequently, everything seemed to indicate that the Park's boundaries could still be modified. Furthermore, in the area of Km 53-54, the proposed boundary passed right through an unnamed lake. **Consultations with Messrs. Gagnon and Lamarre of the MDDEP – Direction du patrimoine écologique also suggested that the boundaries of the future Assinica Park would lie in the same corridor as Hydro-Québec's power transmission line.**

On the basis of this information, **it was decided to follow the power transmission line so as to minimize the ecological footprint** of Barrette-Chapais Itée's proposed road.

In light of a private consultation with Messrs. Gagnon and Lamarre of the MDDEP (see report in Appendix 7.1), it appears that the boundaries of the future Assinica Park are still subject to modifications. A provisional map of the boundaries of the future Park was shown during this consultation (it is not possible to reveal the boundaries in this document for reasons of confidentiality): it appears that a significant slice has been removed from the western area of the Park and that a corresponding area has been added in its eastern portion.

Thus, **the corridor proposed by Barrette-Chapais currently lies outside the boundaries of the future Assinica Park.**

- 7.2 Question 22: The project proponent will add the boundaries of the future Assinica Cree Heritage Park to its Map 5•1 (Potential gravel pits) to allow the identification of unusable borrow pits.

Maps 7.2 South and 7.2 North of the initial document show, firstly, the anticipated boundaries of the future Assinica Park. In addition, all the borrow pits that Barrette-Chapais Itée plans to use to build the road are indicated on these maps using reference numbers. Section 8.2 explains in greater detail the planned use and actual potential of these borrow pits.

These maps therefore show more accurately the locations where Barrette-Chapais Itée wishes to extract gravel in order to build the road.

Based on the information appearing on these maps, **only borrow pit #1 might fall within the boundaries of the future Assinica Park.**

INSERT MAP 7.2 SOUTH

INSERT MAP 7.2 NORTH

8 General information

- 8.1 Question 3: According to Maps 7•3, 7•4, 7•5 and 7•6 (Close-ups views: Ecological map), the logging road will not follow the same corridor as Hydro-Québec's road but rather lie parallel to it. Is that really the case? If so, please give the reasons for taking this approach and indicate whether special rehabilitation measures are planned.

At the planning stage, a preliminary corridor was drawn using the standard mapping tools used for this type of work. The proposed corridor was based on available geographical information regarding the location of the power transmission line and the road running under it. This information was digitized using MRNF maps at a scale of 1:500,000. Consequently, these computational tools contain inaccuracies that may cause distortions of the data. The corridor proposed by Barrette-Chapais Itée has also been drawn using a map at the same scale: consequently, it may contain the same distortions.

The actual alignment of the road may therefore be somewhat distorted with respect to the desired alignment, in certain locations, due to inherent limitations of available tools.

In choosing the road's alignment, the objective is to have the road lie within the right-of-way of the power transmission line and follow the existing road as much as possible. However, several physical constraints need to be taken into consideration. In fact, in several locations, the road zigzags under the power transportation line. In certain places, it also passes under the power line. **The road proposed by Barrette-Chapais Itée will ensure that the road's alignment is as straight as possible and remains west of the power transmission line. Thus, curves of the existing road will be eliminated and, whenever the existing road crosses under the power line, the planned road will pass west of the line and never directly under it.**

Finally, an additional constraint concerns the existence of stays that maintain Hydro-Québec's towers in place. In order to ensure the integrity of the power transmission line's support structure, **the road will not come within less than 10 metres from the stays that support Hydro-Québec's towers.** This will ensure that no accident involving either an oversized vehicle or a normal vehicle leaving the road shall endanger the structural integrity of the existing tower stays.

- 8.2 Question 18: Regarding the caption of Map 5•1 (Potential gravel pits), the project proponent will supply information regarding the meaning of references 1BF, 1BG, 2A, 2AE, 2AK, 2BE and 9S. In addition, the project proponent will identify which of these sites hold the most potential for the construction and maintenance of a logging road.

The caption was deliberately omitted from the initial document in order to make it easier to read. To answer the question regarding the caption for surface deposits and to make

interpretation easier, Table 8.2.1 sums up all the information required to correctly interpret the information provided.

Table 8.2.1: Excerpts from the descriptions and codifications of surface deposits.

Source: Robitaille, A. 1988. *Cartographie des districts écologiques : normes et techniques*, Ministère de l'Énergie et des Ressources, Service de l'inventaire forestier, Division écologique, Québec, 109 pages.

Type of deposit	Map code	Description	Origin and morphology
1. Glacial deposits		Loose or compact unsorted deposits, composed of rock flour and subangular to angular components. The size distribution of the sediments varies from clay to boulders depending on the region	Deposits left by glaciers, without any major meltwater contribution, as a result of bedrock erosion. They can present a wide range of topographical characteristics.
1.2 Glacial deposits characterized according to their morphological features		These glacial landforms are in general composed of till.	These glacial deposits stem from the action of glaciers. They are sufficiently thick to create landforms over rock or loose formations.
End moraine	1bf	End moraines are composed of large accumulations of glacial sediments: sand, gravel, boulders. Deposits are stratified in certain locations, while they do not feature any visible sedimentary structure elsewhere.	This type of moraine, formed at glacial fronts, marks precisely the ancient margin of a glacier. They may reach several tens of metres in height and hundreds of kilometres in length.
De Geer moraine	1bg	The small ridges that form a De Geer moraine are composed of till, sometimes bleached superficially, generally rocky and sometimes covered with boulders or gravel.	This type of moraine is deposited in shallow bodies of water at glacial fronts. Its main topographical feature consists of small ridges (3-10 m) parallel to the glacial front.
2. Glaciofluvial deposits		Glaciofluvial deposits are composed of heterometric sediments whose shape varies from subrounded to rounded. They are stratified and may contain pockets of till (flowtill).	Deposits left by glacial meltwater. The morphological features of the deposits vary in accordance with the proximity of the depositional environment and the glacier (ice-contact and proglacial deposits)
2.1 Ice-contact deposits	2a	Deposits composed of sand, gravel, pebbles, rocks and, sometimes, boulders, with shapes that vary from subrounded to rounded. They often present a distorted and faulted stratification. Component size distribution varies significantly from stratum to stratum. Such deposits often contain till pockets.	Deposits left by glacial meltwater at the margin of retreating glaciers. They often have a hummocky topography dotted with kettles.
Esker	2ae	Idem	Eskers form in supraglacial, englacial or subglacial streams when a glacier melts. They take the form of elongated ridges, which may be rectilinear or sinuous and continuous or discontinuous.
Kame	2ak	Idem	Kames form through the accumulation of sediments in a depression caused by a stagnant glacier. After the ice melts, they appear as hummocks or low mounds, of varying heights, with steep slopes.
2.1 Proglacial deposits	2b	Proglacial deposits are mainly composed of sand, gravel and rounded pebbles. These sediments are sorted and deposited in distinct layers. In a complex, particle sizes decrease from upstream to downstream.	Such deposits are deposited by glacial meltwater and glaciofluvial streams.

Outwashes	2be	Idem	Deposits left along glaciofluvial streams. The generally flat and uniform surface of the outwash is creviced where ancient drainage channels ran. Glaciofluvial terraces located along current rivers often correspond to eroded residual outwashes.
9. Aeolian deposits		Deposits usually composed of bedded and well sorted sand, with varying particle size distributions, from fine to medium grade.	Deposits take the form of elongated or crescent-shaped hummocks, built by the wind.
Anchored dunes	9	Idem	Deposits whose form is protected from further wind action by a stabilizing growth of vegetation.

Reminder: the boundaries of the future Assinica Park are currently being negotiated with the Cree community and are subject to modification. Thus, although we have had access to the currently proposed definition of the boundaries and it is possible that a significant slice would be subtracted from the western part of the future park, we have considered it advisable and a more conservative assumption to take the boundaries to be those presented in the first document

Accordingly, in answer to the question regarding the actual potential for using the identified borrow pits, Maps 7.2 South and 7.2 North show, as explained in section 7.2, the anticipated boundaries of the future Assinica Park that were presented in the first document. Furthermore, all the borrow pits that Barrette-Chapais Itée plans to use to build the road are indicated on these maps using reference numbers. Each borrow pit bears a number that makes it possible to retrieve its ecological and morphological characteristics. Table 8.2.2 summarizes the individual ecological characteristics and compositions of all borrow pits. However, no test boring has been done in order to validate the quantity and quality of the material.

Table 8.2.2: Summary of the individual eco-forestry characteristics of all potential borrow pits for the extension of road L-209-N by Barrette-Chapais Itée.

Borrow pit #	Forest type	Average stand age	Morphological characteristics	Area (ha)
1	Jack pine forest	70	2be	4.78
	Jack pine forest	70	1a	0.46
	Jack pine forest	70	1b	22.2
	Jack pine forest	90	1a	2.0
	Jack pine forest	Old uneven-aged stand	2be	1.1
	Total			30.54
	Total			
2	Poplar and pine forest	70	2ae	7.0
	Spruce stand	70	2a	13.6
	Spruce stand	70	2a	1.7
	Jack pine and spruce stand	70	2a	5.0
	Spruce forest	70	2a	23.5
	Spruce forest	70	2be	8.1
	Total			58.90
3	Jack pine and spruce stand	70	2A	39.8
	Total			
4	Spruce stand	120	1a	5.2
	Spruce stand	90	2a	3.8
	Jack pine plantation	10	1a	5.9
	Total			14.90
5	Spruce stand	70	2be	2.2
	Jack pine and spruce stand	70	2a	8.5
	Spruce forest	70	2a	10.4
	Total			21.10
6	Spruce stand	120	2a	29.0
	Spruce stand	120	2a	1.4
	Jack pine and spruce stand	120	2a	0.1
	Jack pine and spruce stand	70	2a	6.0
	Spruce forest	70	2be	0.2
	Spruce forest	70	2a	6.1
	Total			41.40
7	Jack pine forest	26	2a	15.2
	Burn	26	2a	13.4
	Total			28.60
8	Jack pine and spruce stand	70	2a	3.1
	Jack pine forest	23	2a	13.8
	Jack pine forest	23	2ae	12.8
	Total			29.70
9	Burn	23	2ae	23.4
	Jack pine forest	23	2a	9.9
	Total			33.30
10	Jack pine and spruce stand	120	2ae	8.0
	Jack pine plantation	10	2a	0.2
	Burn	23	2a	13.3
	Cutover	10	2ae	5.4
	Strip cut	10	2a	2.6
	Total			29.50
11	Spruce forest	120	2a	16.5
	Burn	23	2a	8.3
	Total			24.80
12	Jack pine and spruce stand	70	1a	2.7
	Jack pine and spruce stand	70	2A	4.0
	Jack pine and spruce stand	70	2AE	8.9
	Spruce forest	70	2A	8.2
	Spruce forest	70	2A	1.9

	Total				
13	Jack pine and spruce stand	70	2A	6.4	25.70
	Spruce forest	70	2AE	10.5	
	Spruce forest	70	2A	1.6	
	Total			18.50	
TOTAL					396.74

- 8.3 Question 19: The project proponent will supply a map of the vegetation cover of potential borrow pits.

Maps 8.3 South and 8.3 North show the vegetation cover of each borrow pit identified by Barrette-Chapais Itée.

- 8.4 Question 20: The project proponent will indicate the wildlife habitat potential of the borrow pits (including for the caribou and the moose).

The potential wildlife of the region includes, among other species, the moose and the caribou.

- **Moose**

The moose is a species that relies on a variety of habitats for feeding, calving and protection. This species use of these habitats varies according to the period of the year. It should be noted that its habits vary according to two main periods, i.e. the summer and the winter periods.

In summer, the moose relies on high density habitats to escape the intense heat. It also uses open habitats where the food density is high (Sansou et al, 2002).

In winter, this large mammal uses habitats that present a dense vegetation cover. This feature allows it to move more easily due to the thinner snow cover found there.

- **Woodland caribou**

Woodland caribou occupy mostly mature softwood habitats. Indeed, the team responsible for the recovery of woodland caribou in Quebec (2008) indicates that the usual habitat of woodland caribou consists of mature black spruce stands with or without lichen. This is the habitat that is occupied most of the time by this species.

In light of the information regarding the habitats of these two species, the planned extension of road L-209-N by Barrette-Chapais Itée will modify the area of potential habitats for these two species.

However, based on the average age of the stands found in the areas of potential borrow pits, we note that **the characteristics of locations # 7-10 are unfavourable with respect to offering either shelter or feeding stands for both the moose and the woodland caribou.**

INSERT MAP 8.3 SOUTH

INSERT MAP 8.3 NORTH

All these locations have undergone either recent logging or a forest fire in the last 25 years. Conversely, **locations #4, 6 and 11 are interesting potential wildlife habitats for the moose and the woodland caribou.** All these habitats are characterized by a high average age, i.e. 90 years and greater.

Although these areas are likely shelter or feeding habitats for these two species, such habitats are also characteristic of the region. Furthermore, the majority of the borrow pits in question are located close to the proposed road corridor.

- 8.5 Question 21: *The project proponent will supply the locations of potential borrow pits that it plans to use for the construction and the maintenance of the logging road.*

All the borrow pits identified in **Table 8.2.2** and shown on **Maps 7.2 South and 7.2 North are sites** that Barrette-Chapais Itée will use **for the construction** of the extension of road L-209-N.

- 8.6 Question 30: *The project proponent will indicate how provisions of the Paix des braves agreement concerning sites of interest that have been identified and mapped by Cree tallymen (1%) and factors linked to the preservation or improvement of the habitats of wildlife species considered important by tallymen (25%) may have influenced the choice of the logging road's corridor.*

While planning the road, Barrette-Chapais Itée held private consultations with trappers affected by the construction of the planned road. This procedure is required by the Paix des braves agreement. **Furthermore, a “Land-use conflict” resolution procedure was initiated under the leadership of the MRNF.** The reports on these two meetings – held December 4, 2008 and January 15, 2009 – were used to prepare the alignment that is proposed in the assessment. These reports can be found in Appendix 8.6 of this document. Following these consultations, the 2008-2013 General Forest Management Plan was submitted, integrating the aspects of the proposed corridor that were agreed upon by stakeholders. After the plan was submitted, the MRNF analyzed the road's proposed corridor with respect to its impact on both tallymen and wildlife.

All requests regarding the fact that the original corridor sometimes crossed over specific areas targeted by the tallymen (1% and 25%) have been taken into account in the choice of the final corridor of the road under study.

- 8.7 Question 31: *Has the project proponent considered, together with the appropriate authorities, the possibility of installing a panel on the side of the logging road at the entrance to Category II lands informing the public of the rights and privileges guaranteed to the Cree by the James Bay and Northern Quebec Agreement with respect to sport fishing and hunting?*

The possibility of installing a sign at the border of Category II lands was raised during discussions with tallymen. Concerned individuals were favourable to this idea. It is

therefore agreed that **Barrette-Chapais Itée or the MRNF - Direction de la Faune shall install signs informing road user that they are entering area that are classified as Category II lands** according to the James Bay Agreement. Such panels will state wildlife management regulations applicable in the area and procedures that must be followed.

8.8 Question 32: The project proponent will describe the action plan that it intends to implement in order to respond to complaints submitted by users of the logging road.

The construction of a road as proposed by Barrette-Chapais Itée could entail temporary modifications of the habits of current users of the road that runs under the power transmission line. Consequently, some users might feel adversely affected or have complaints they wish to submit to those responsible for the construction of such road.

To facilitate communication between users of the existing road and the project's proponent, tools to formulate comments or complaints will be developed before the start of construction.

Such tools will include, among others, **a suggestion box** at the start of the road where individuals will be able to submit comments regarding the project. In addition, at the same location, a description of the **procedure for making a complaint will be made available to visitors. It will include the name, phone number and email of a contact to which their complaints may be addressed.**

Once a complaint or suggestion is received, the person in charge of the monitoring and follow-up program will perform a first check in order to confirm the validity of the complaint. Should the complaint be substantiated, **immediate corrective action will be taken** in order to fulfil the expectations of other users. Finally, **a register of complaints**, indicating whether such complaints were substantiated and how they were processed, shall be maintained at the offices of Barrette-Chapais Itée and will be made available for inspection by officials of the MDDEP and the MRNF. This register will record complaints made in the last five years. Finally, **the above-described procedure will be incorporated into Barrette-Chapais Itée's ISO 140001-2006 Environmental Management System** in order to ensure its application.

8.9 Question 33: The project proponent will indicate when it intends to submit the English version of its impact assessment.

The English translation of the document entitled « Étude d'impact social, économique et sur l'environnement de la région de Chapais et Chibougamau : Construction d'un chemin forestier de transport de bois pour camion hors norme » and the responses to the supplemental questions will be translated soon. **We intend to present these translations 45 days after the date at which this document is submitted.**

9 Woodland caribou

- 9.1 Question 4: Given that the issues surrounding the construction of two variants of the corridor in the Lake Ruth area are strictly of biological nature (i.e. the presence of a listed herd of woodland caribou – within the confines of a planned wildlife reserve – versus concern over the possibility of sports fishing in Lake Caupichigau), why did the project proponent not rely on biological criteria to select the most appropriate corridor?

The starting assumption that led to the choice of the alternate corridor for the construction of the extension to road L-209-N rests on biological considerations supported by social considerations.

The assumption is that routing the road on the east of Lake Ruth would push the woodland caribou herd deeper inside the boundaries of the future Assinica Park.

Excerpt from the impact assessment:

"The road would therefore be on the western border of the future Assinica Park. Given the presence of a large protected area such as Assinica Park, it may be assumed **that the woodland caribou population will migrate towards old growth forests located inside the boundaries of the future park**. However, it was not possible to verify this hypothesis by consulting the literature. **As previously mentioned, the alternate corridor would subtract an area of 30 km² from the home range of the woodland caribou population of Lake Ruth.** It is assumed here that the caribou would cease to visit the shores of Lake Ruth if the road were constructed. **However, this 30 km² area is in reality already degraded by the presence of the road built by Hydro-Québec along the eastern shores of Lake Ruth.**"

We also are aware that the Waswanipi community has many concerns over the passage the road west of Lake Ruth (Appendix 10.8). **During public consultations held last June, the issue of opening up Lake Caupichigau to non-natives sport fishermen was raised. Such concerns were expressed in a very emotional way.**

Thus, consideration of anticipated biological impacts alone appeared to favour the corridor passing west of Lake Ruth, but, **in light of the combined environmental and social impacts of the project, the editorial team that drafted the impact assessment felt compelled to support the route passing east of Lake Ruth.**

In fact, it should be noted that it is very likely that the woodland caribou herd will move deeper into the boundaries of the future Assinica Park, which could end up ensuring its survival. On the other hand, opening up Lake Caupichigau would be irreversible.

Finally, private consultations with the tallymen responsible for areas #M47A, O52 and O53 reveal that the passage east of Lake Ruth ought to be favoured, in light of the fact that Mr. Simeon Mianscum (W12) considers that the damage caused by such an increased access would be so great as to jeopardize future traditional activities on this family hunting ground.

Consequently, the choice between the routes east or west of Lake Ruth became Mr. Simeon Mianscum's decision. A meeting was held with Mr. Mianscum on December 14, 2009, in order to learn his position regarding this issue. All the reports on these meetings can be found in Appendix 9.1 of this document.

Mr. Mianscum indicated that the route passing west of Lake Ruth would cause water quality degradation, increase fishing pressure on Lake Caupichigau and facilitate access to family hunting ground #W12. **He also indicated that the opening up of the territory would be irreversible and could not be compensated in any way.** Finally, he indicated that the presence of woodland caribou east of Lake Ruth did not justify opening up his family hunting territory.

However, Mr. Mianscum also indicated that the road would give him the opportunity to build a new permanent camp on the shores of Lake Caupichigau, something that he viewed as a positive outcome for his family.

At the end of the meeting, Mr. Mianscum indicated that, should it be impossible to proceed otherwise, he could not oppose the construction of the road along the corridor that passes west of Lake Ruth.

Given the information provided by both Mr. Mianscum and other trappers during these private consultations, as well as the potential impacts of building a road close to the woodland caribou herd east of Lake Ruth, the editorial team recommends that the main corridor (west of Lake Ruth) be chosen. In addition, Barrette-Chapais Itée supports this position.

9.2 Question 5: *Given that the project proponent will in all likeliness not be able to use its logging road more than 1-3 weeks during the fall and winter period, and given that the proponent proposes to limit its use the road to this period of the year in order to minimize disturbances to the woodland caribou herd, what are the advantages of building the road along the alternate corridor, which passes east of Lake Ruth?*

In the first impact assessment document, it was mentioned that Barrette-Chapais Itée would evaluate the possibility of using the road during the fall and winter period (p. 66). We also refer the reader to Barrette-Chapais Itée's commitments regarding this specific aspect of timber transportation in the fall and the winter.

Excerpt from the impact assessment:

"However, the company is committed to evaluating the possibility of transporting timber in the fall or in the winter when conditions permit it. The first evaluations indicate that it might be possible to use the road in the fall or in the winter, over a period of one to three weeks, thus further reducing disturbances to the woodland caribou herd. This way of proceeding might avoid causing disturbances likely to increase predation during the most vulnerable periods (gestation and calving interval)."

Though this commitment does not limit Barrette-Chapais Itée's operations, the company is committed to evaluating the possibility of concentrating its timber transportation operations in the fall or the winter. However, this does not imply that no transportation

operations will take place outside of such periods. It is mentioned that the company will strive to transport a minimum quantity of timber during such period and that **the 1-3 week period could be extended if conditions allow harvesting of larger quantities of wood in the fall or winter.**

10 Private consultations

10.1 Question 6: Which mitigation measures could be considered in order to restrict access to Lake Caupichigau by sport fishermen? In particular, would it be possible to landscape the area near the bridge or the river shore at water crossing #10B so as to impede the launching of motorboats at this location? And, would it be possible to build a motorboat launching ramp on 1 or 2 other water bodies of this area in order to divert fishermen's attention from this spot?

As explained in this document, the choice of the alternate corridor, i.e. the corridor passing east of Lake Ruth, was favoured for a long time. In particular, this decision was justified by the backwards shift of the boundaries of the future Assinica Park.

Discussions were held with representatives of the wildlife section of the MRNF in order to evaluate the feasibility of restricting access to Lake Caupichigau. These discussions led to the **conclusion that it would be very difficult, perhaps impossible, to voluntarily restrict access to the lake**. Any attempt to voluntarily restrict access to the lake would violate the Act respecting the conservation and development of wildlife. **In fact, section 1.4 of the act prohibits any hindrance of activities such as hunting, fishing or trapping.** This correspondence can be found in Appendix 10.1.

Section 1.4 of the act:

"1.4. No person may knowingly hinder a person who is lawfully carrying on an activity referred to in the first paragraph of section 1.3, including an activity preparatory to such an activity."

For the purposes of the first paragraph, "hinder" means, in particular, preventing access by hunters, fishers or trappers to a hunting, fishing or trapping area to which they have lawful access, damaging a hunter's tree stand or field blind, disturbing or frightening an animal or fish by human, animal or any other presence, a noise or an odour, or rendering ineffectual any bait, decoy, gear, trap or implement used to hunt, fish or trap that animal or fish."

Consequently, it would be impossible to restrict access to Lake Caupichigau as suggested by the Review Committee (REVCOM).

Regarding the possibility of building new launching ramps on other water bodies, such as Lake Omo, several concerns were raised by the wildlife section of the MRNF (see correspondence in Appendix 10.1). **The construction of launching ramps on major water bodies could have a pernicious effect by promoting the development of leisure projects,** which would amplify pressure on the territory.

Once again, it should be noted that Mr. Mianscum no longer opposes the choice of the main corridor even though it still entails the most evident set of social, economic and environmental impacts, and the alternate corridor would hinder access to the territory in question.

10.2 Question 7: Given that sport fishing is under the jurisdiction of the MRNF, the project proponent will consult this Department in order to establish the desirability of restricting sport fishing on Lake Caupichigau and, if applicable, the type of measure that could be taken towards that end.

The response to this question is discussed in the previous section.

10.3 Question 8: The project proponent must establish at the impact assessment stage the corridor on which he intends to build the logging road. Once the MDDEP issues a certificate of authorization, if any, the chosen corridor may not be modified without prior authorization from the provincial administration (MDDEP). On the other hand, given that the proponent may develop the area along the logging road in order to accommodate the needs of various users of such road (parking spaces, portage points, snowmobile crossings...), he will indicate whether such developments have been the subject of specific requests from the people that were consulted and the types of developments that are planned, if any.

As mentioned in the first impact assessment document regarding the construction of the road under study, private consultations with tallymen affected by the project were planned.

Such private consultations were effectively held for each affected trapping area. The reports on such consultations can be found in Appendix 9.1.

During those consultations, information was exchanged on several subjects, including **in particular: the areas near water crossings, the repair of class 3 roads leading to given camps, the existence of spawning grounds that might be affected by the project.**

Maps 10.3 South and North show the numbered locations of points for which information or special requests were submitted by the trappers that were consulted.

Table 10.3 details for each trapping area the information or requests submitted during private consultations.

INSERT MAP 10.3 SOUTH

INSERT MAP 10.3 NORTH

Table 10.3: Requests or information submitted during private consultations with tallymen affected by the planned extension of road L-209-N by Barrette-Chapais Itée.

Trapping area (# and name of trapper)	Map ref. #	Information or requests submitted during private consultations
O53 Charlie Bosum	1 and 2	Request that the road leading to Lac à l'eau noire be finished so that he may have complete access to his main camp.
	3 and 4	Mr. Bosum explained that the extension of road L-209-N will pass near his brother's camp (3) and that the condition of road (4) leading to this location has degraded; Mr. Bosum therefore requested that road (40 be improved.
	5	In the past, moose could be found in large numbers at this location; however, since salvage logging operations were carried out here to harvest fuel wood, it no longer holds any special interest.
	6 and 7	Mr. Bosum indicated that the approach to the bridge over the Brock River (6) was dangerous and requested that the bridge be relocated (7).
	8	Walter Bosum (son of Charlie) mentioned that hydrocarbon tanks dating back to the construction of Hydro-Québec's power transmission line might be buried here.
O52 David Bosum, Sr.	9 and 10	M. Mr. Bosum identified 2 lakes located west of the corridor that would be affected by the project.
	11 and 12	Mr. Bosum requested that the road leading to Lake Waposite be repaired so as to give him easier access to his camp.
	13	At Lake Ruth, there is a Canada goose resting area used by the Bosum family, but they will not be disturbed by the road's construction.
	14	Mr. Bosum mentioned that oil containers are buried under the old Hydro-Québec camp site.
M47A Charlie John Coon	15	Construction of an access road leading to Lake Capiehigamau.
	16	Request to move camp to Lake Capiehigamau via the requested access road.
	19	Request that a barrier be installed at the entrance to the new road in order to restrict access to Lake Capiehigamau.
	17	Presence of a spawning ground. Culvert #2009-26: the species remains confidential.
	18	Mentioned the existence of an old camp and expressed his wish that no access be given to the two lakes near this camp.
	20	He explained that Lake Capiehigamau is uncontaminated and expressed his wish that the road's construction would not affect water quality in this lake.
W10A Johnny Trapper	21	Construction of a new road leading to a point further south of Lake Omo
	22 and 24	Repair of the road leading to Lake Omo
	23	Opening of the old Hydro-Québec household waste dump site.
	25-27	Presence of woodland caribou tracks.
	28 and 29	Presence of spawning grounds (species: confidential).
	30	Location of Mr. Trapper's main camp.
	31 and 32	Snowmobile trail.
W22 Willy Wapabee	33	Location of Mr. Wapabee's temporary camp.
	34	Removal of snow from this road before Goose Break.
	35	Bridge over the Lake Thomelet river
	36	Location of Mr. Wapabee's permanent camp.
W12 - Simeon Mianscum	36 and 37	Repair of the road leading to his camp.
	38	Location where a new camp is to be built on the shore of Lake Caupichigau.
W10 Joseph Neeposh	39	Repair of the road leading to his father's camp.
	40	Repair of the road leading to his camp.
	41	Installation of a sign indicating that this is Category II land.
	42	Protection and preservation of the access to the water spring that he uses.
	43	Presence of a spawning ground (species: confidential).
	44	Area where woodland caribou is present.
	45	Burial site on Lake Caminscanane
	46	Snowmobile trail.
	47	Canoe portage.
	48 and 49	Temporary camps.

Table 10.3 sums up all the **information and requests submitted during private consultations** with tallymen. Overall, the table reveals that trappers have **special needs regarding the repair of roads leading directly to certain camps**. Furthermore, certain locations have been identified as potential spawning grounds, but trappers do not wish their exact location or the species involved to be specified.

As explained above, in light of the recommendations of the editorial team and following discussions held with Mr. Mianscum, Barrette-Chapais Itée favours the main corridor in spite of the more significant social impacts this choice entails for the sake of protecting the woodland caribou herd.

10.4 Question 15: The project proponent will hold consultations with tallymen regarding the anticipated impacts of the logging road on spawning grounds known to them.

Consultations were held with tallymen as discussed above. In all cases, a specific question was asked in order to elicit any specific knowledge the trappers may have regarding spawning grounds that may be affected by the project.

It emerges from these consultations **that there are three spawning grounds that may be affected by the project**. In order to protect and guarantee the confidentiality of this information, locations have only been identified if the trapper gave his permission. A special effort will be made to validate information provided by tallymen. The species and location of the spawning grounds will need to be validated by officials of the wildlife section of the MRNF or by qualified personnel accompanied by the tallymen.

In the case of family hunting area #**M47A**, the location of a spawning ground has been identified. This spawning ground, located upstream of the road, could be affected by the construction of the road. Special care will be taken to minimize the influx of sediment into the river. Furthermore, depending on the species, construction work at this river crossing will be performed outside the spawning run time of the species in question.

In family hunting area #**W10A**, a spawning ground is also believed to exist. Once more, this spawning ground will need to be confirmed by qualified personnel; should its existence be substantiated, construction work will be performed outside spawning season (May-June). Furthermore, sedimentation and hydrocarbon barriers will be installed in order to preserve water quality during construction work.

Finally, in area #**W10**, there is a spawning ground of a species that is kept confidential, downstream of the road. As for areas #M47A and W10A, the proposed measures will be applied in order to protect this spawning ground.

- 10.5 Question 16: Given the above-mentioned information, the project proponent will validate with the MRNF to what extent its road construction project is compatible with the development plan in preparation. To this effect, the proponent will specify, among other things, the relative magnitude of the home range affected by the project compared to the substitute territories made available through the proposed creation of the Assinica Cree Heritage Park. The proponent will also indicate any existing data regarding the size of this population and its vulnerability to the project, and specify any proposed mitigation measure.

At the time this document was being drafted, the MRNF – Direction de la faune had not yet stated a clear and uniform position regarding logging road construction activities in the whole of the North of Quebec.

Several logging road construction projects are in preparation and this means that the MRNF has doubts regarding the appropriate position to take regarding the woodland caribou. Thus, it was impossible for the Department to state a position as requested by Barrette-Chapais Itée regarding the planned extension of logging road L-209-N.

The MDDEP will be informed immediately of the MRNF's position as soon as it is communicated to Barrette-Chapais Itée.

It is expected that this position will be finalized at the start of 2010.

- 10.6 Question 29: The project proponent will hold new consultations with tallymen affected by the project in order to obtain information regarding their use and occupation of the land, including: the location of permanent and temporary camps, burial grounds, portage and snowmobile trails, special interest sites that require protection, etc.

Maps 10.3 South and North show the numbered locations of temporary or permanent camps, burial grounds and family trails that have been mentioned by tallymen.

The road never crosses any such sensitive areas. Furthermore, several trappers have mentioned that the Barrette-Chapais Itée's new road will provide them with easier access to their territory. In certain cases, the trappers have also expressed the wish that Barrette-Chapais Itée prepare the surfaces on which they plan to install new permanent camps.

- 10.7 Question 27: The project proponent will supply any information regarding the logging road project obtained from the Oujé-Bougoumou tallymen by the Traditional Pursuits Director.

As discussed above, private meetings were held with each tallyman affected by the planned construction of the extension to road L-209-N. In particular, private consultations

were held with two representatives of the Cree community of Oujé-Bougoumou, i.e. Messrs. Charlie and David Bosum. During these meetings, specific information was requested. The report on these meetings can be found in Appendix 9.1.

These meetings helped to clarify the Oujé-Bougoumou community's position regarding the main corridor (passing west of Lake Ruth) and the alternate corridor (passing east of Lake Ruth). A presentation was made to ensure that participants were aware of the dilemma surrounding the routing of the road around Lake Ruth. In all cases, the community representatives indicated that Mr. Simeon Mianscum's concerns were justified. If such a situation presented itself on their territories, they would adopt a similar attitude. Furthermore, they indicated that they had never seen any woodland caribou in the area in question and they expressed surprise at the thought that a herd of this animal should be found close to a power transmission line.

In conclusion, Messrs Bosum expressed solidarity with Mr. Simeon Mianscum's position and declared that they would not object if the road were to pass east of Lake Ruth should Mr. Simeon Mianscum maintain his request to this effect.

10.8 Question 28: As requested in the instructions, the project proponent will supply a report of each consultation meeting that was held, including in particular: its date and location, the list of participants (or number of participants), the list of entities represented, the subjects discussed and the opinions expressed.

Appendix 10.8 contains all the reports regarding public and private pre-consultations that were held in preparation of the drafting of the first document.

11 Bridges and watercourse crossings

11.1 Question 9: For all water crossings currently accessible by road, the project proponent will supply an analysis characterizing the receiving body of water, including: the width of the watercourse being crossed, its depth, water conditions (rapids, rocky bottom...), shore conditions (substrate, height, vegetation), and whether the presence of fish is possible or has been confirmed. Furthermore, the proponent will specify the type of the structure to be installed, so as not to reduce the width of the watercourse by more than 20%. And, for all water crossings that are not accessible by road, the proponent will perform the same operation using aerial photography or an overflight. In all cases where it will not be possible to follow the 20% encroachment rule, the project proponent will supply information regarding the selection of the structure that it plans to install.

All the watercourses that are crossed by the planned logging road have been visited, either by road or by overflying their location. This allowed information to be gathered regarding their characteristics. **Table 11.1 sums up the information gathered regarding each water crossing point.**

Table 11.1: Information (physical characteristics, proposed structures and geographical coordinates) regarding each watercourse crossed by the planned extension of road L-209-N by Barrette-Chapais ltée

Business number : 100403336	Geographical coordinates		Width of the watercourse (m)	10-year interval peak flow (m ³ /s)	Physical characteristics of the watercourse	Structures proposed by Barrette-Chapais ltée	Watercourse restricted by more than 20%?
	Longitude	Latitude					
2009-01	75°15'31.897	49°59'06.813	1	0.39	Soft bottom; bordered by alders	1 * 800	No
2009-02	75°19'31.678	50°00'22.040	3	1.46	Soft bottom	1 * 1400	No
2009-03	75°21'15.194	50°00'53.627	1	0.45	Soft bottom; bordered by alders	1 * 800	No
2009-04	75°26'50.671	50°01'39.217	3	0.95	Soft bottom	1 * 1200	No
2009-05	75°28'14.419	50°03'31.991	1	0.35	Soft bottom	1 * 800	No
2009-06	75°28'20.491	50°03'42.937	43	30.35	Rocky bottom	Bridge	No
2009-07A	75°28'35.942	50°04'36.908	4	1.64	Soft bottom	1 * 1400	No
2009-08A	75°28'46.245	50°05'36.906	1	0.27	Soft bottom - flooded width: 15 m	1 * 800	No
2009-09A	75°28'55.635	50°08'13.599	1	1.20	Soft bottom	1 * 1200	No
2009-10A	75°28'17.330	50°09'40.757	2	1.05	Soft bottom	1 * 1200	No
2009-11A	75°28'09.992	50°11'14.686	5	15.93	Soft bottom	2 * 2700	Yes
2009-12A	75°29'32.089	50°12'09.913	4	0.24	Soft bottom - flooded width: 15 m	1 * 800	No
2009-13A	75°29'57.999	50°13'28.164	1	0.22	Soft bottom	1 * 600	No
2009-09B	75°29'45.295	50°08'00.846	2	1.20	Soft bottom	1 * 1200	No
2009-10B	75°31'02.179	50°09'16.415	12	21.42	Soft and rocky bottom	Bridge	No
2009-11B	75°31'26.360	50°09'48.655	1	1.49	Soft bottom	1 * 1400	No
2009-12B	75°32'02.599	50°11'30.658	1	0.30	Soft bottom	1 * 800	No
2009-13B	75°31'37.951	50°12'05.054	4	0.68	Soft bottom	1 * 1000	No
2009-14	75°30'01.444	50°15'06.172	3	4.13	Soft bottom	1 * 2000	No
2009-15	75°30'02.730	50°15'33.825	1	1.40	Soft bottom; bordered by barren wetland	1 * 1400	No
2009-17	75°30'03.550	50°15'51.502	2	1.59	Soft bottom	1 * 1400	No
2009-18	75°30'06.074	50°16'10.403	5	15.79	Soft and rocky bottom	1 * 3300	No
2009-19	75°30'39.024	50°17'15.191	3	3.98	Soft bottom	1 * 2000	No
2009-20	75°30'37.434	50°22'01.229	6	1.27	Soft and rocky bottom	1 * 1200	No
2009-21	75°30'59.216	50°23'01.974	2	0.58	Soft bottom	1 * 1000	No
2009-22	75°32'02.279	50°24'49.775	1	0.47	Soft bottom	1 * 1000	No
2009-24	75°32'57.312	50°26'15.485	3	0.40	Soft bottom	1 * 800	No
2009-25	75°33'13.966	50°26'29.685	1	0.31	Soft bottom	1 * 800	No
2009-26	75°33'28.680	50°26'41.281	5	19.16	Soft and rocky bottom	2 * 2800	Yes
2009-27	75°34'46.545	50°27'16.714	2	0.99	Soft bottom	1 * 1200	No
2009-28	75°35'15.932	50°27'35.844	5	7.45	Soft bottom	1 * 2700	No
2009-29	75°36'31.454	50°29'14.148	1	0.24	Soft bottom; bordered by barren wetland	1 * 800	No
2009-30	75°39'10.767	50°29'14.148	3	3.47	Soft bottom	1 * 1800	No
2009-31	75°43'24.286	50°30'15.919	1	0.91	Soft bottom	1 * 1200	No
2009-32	75°47'23.196	50°31'19.506	2	1.29	Soft bottom	1 * 1400	No
2009-33	75°48'30.642	50°31'51.614	2	1.64	Soft bottom	1 * 1400	No
2009-34	75°50'07.830	50°32'08.512	16	27.06	Soft bottom	Bridge	No
2009-35	75°52'53.641	50°33'10.147	2	0.82	Soft bottom	1 * 1200	No

A: proposed alternate corridor

B: proposed main corridor

As previously explained in the initial document, the planned road will require two or three bridges depending on which of the main or alternate corridors is selected. The alternate corridor was subsequently selected in light of the discussions, private consultations and the modified boundaries of the future Assinica Park.

Thus, the planned road would follow the alternate corridor, requiring the construction of only two bridges. **In the case of the alternate corridor, two structures will end up reducing the width of watercourses by more than 20%. In each of these cases (crossing #2009-11A and #2009-26), two culverts will be installed** while at the majority of crossings, only one culvert will be used.

At all water crossings where a bridge is not to be used, the culvert will be made of corrugated steel pipe, which is the regular practice in the industry.

The diagram in Figure 11.1.1 shows how a culvert is built.

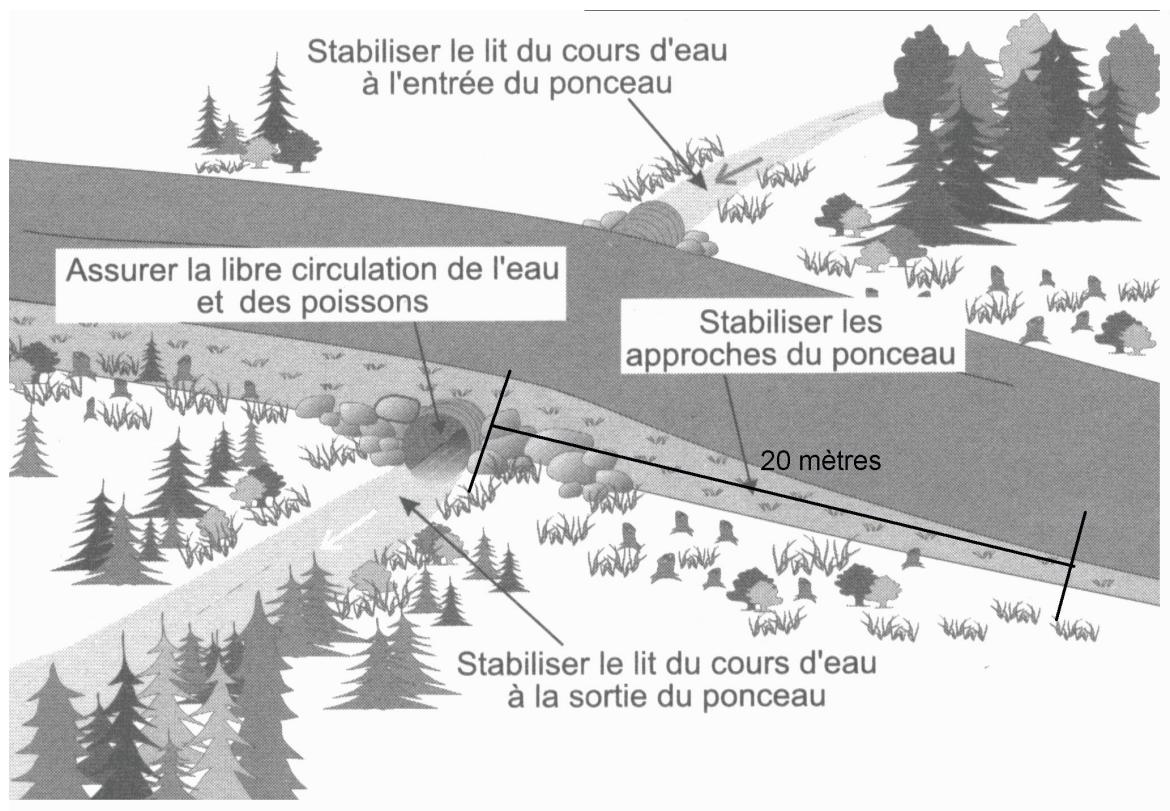
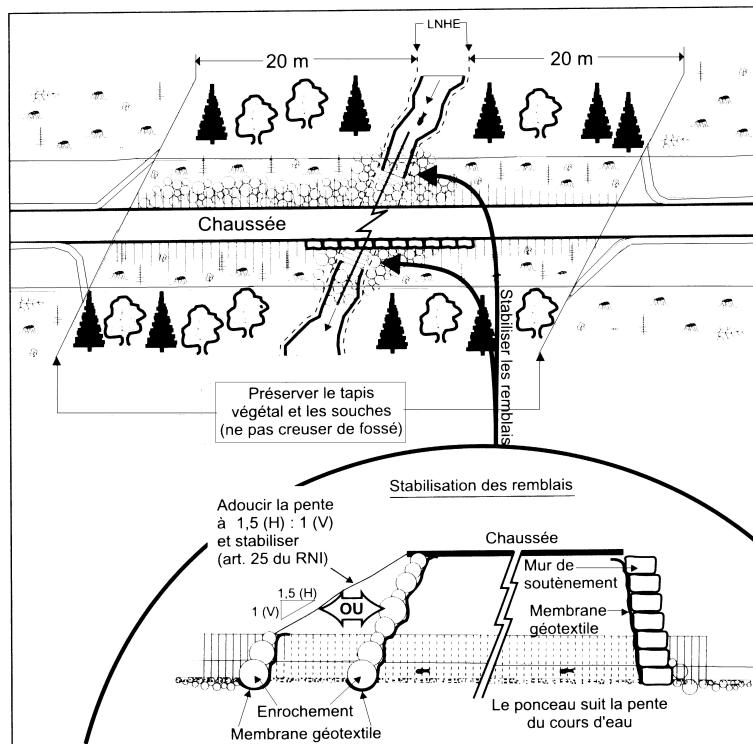


Figure 11.1.2 illustrates the standards that must be followed when a company intends to install a culvert.



- 11.2 Question 10: At crossing #6, can the existing structure installed by Hydro-Québec, considering its width and bearing capacity, be used safely by oversized trucks carrying a 100 ton load? In such case, what work would be required to make this structure safe?

Crossing #2009-06 was installed at the time of the construction of Hydro-Québec's power transmission line in 1975. This crossing comprises three culverts (two 2600 mm culverts and one 1200 mm culvert). These culverts are covered with gravel over a width of 10 metres (photo 11.2).

Currently, the structure allows the passage of normal road vehicles without danger.

The passage of oversized trucks would require significant changes in the load bearing capacity of the structure. In order to make the structure safe, extensive work would be required. It would be necessary in particular to widen the structure of the road itself. **This would entail extending the culverts by at least 3 metres in order to follow construction standards applicable to a structure of this type.** To this effect, it would be necessary to completely dismantle the structure and replace the culverts with parts that conform to the rules.

Therefore, **Barrette-Chapais Itée would prefer to completely dismantle the existing structure and replace it with a bridge**, which would present the advantage of re-establishing the normal flow of this river, thus promoting the free passage of water and fish.



Photo 11.2: Aerial photo of the structure at water crossing #2009-06 – Planned extension of road L-209-N by Barrette-Chapais Itée.

11.3 Question 11: Given the age of this water crossing structure, it is possible that its removal could cause significant environmental impacts, in particular due to the destruction of spawning grounds. The project proponent will specify the real anticipated impacts of the demolition of this structure and compare them with the anticipated benefits of building the planned bridge.

In order to evaluate the feasibility of dismantling this structure, the MRNF – Direction de la faune was asked to provide an opinion on this issue. Correspondence regarding this can be found in appendix 10.1.

This correspondence reveals that the MRNF views favourably the removal of the current structure and its replacement by a bridge. Indeed, it appears that the currently installed culverts do not allow the free passage of fish. Furthermore, according to the observations, the watercourse flowed at a constant rate before the installation of the culverts.

The MRNF has given an opinion regarding the dismantling of the structure. **Departmental officials consider that it would be preferable to replace the water crossing structure with a bridge, even if a spawning ground has formed at that location over time.**

In light of the above-mentioned information, the editorial team believes that it is advisable to build a bridge at crossing #2009-06.

The dismantling of the existing structure will cause a significant modification of the water flow. Furthermore, the removal of the gravel and its replacement by a steel joist structure will cause a large increase in suspended solids in the water. In addition, as explained in the preceding paragraph, it is possible that a spawning ground will be affected by this operation.

Following the same impact assessment procedure as in the initial document (Table 8.1, page 51), four impacts are anticipated. These are:

- Increase in suspended matter in the water
- Potential destruction of a spawning ground
- Restoration of the natural water flow
- Free passage of fish and boats

Table 11.3 sums up the anticipated impacts of the dismantling of the existing structure and of the construction of a bridge.

Table 11.3: Summary of impacts resulting from the dismantling of existing culverts and the construction of a bridge where the planned extension of road L-209-N by Barrette-Chapais Itée crosses Lake Thomelet River.

Impacts	Description	Effects	Intensity	Extent	Duration	Significance
Environmental	Dismantling of the culverts on Lake Thomelet River	Increase in suspended matter in the water	3	2	1	6 (medium)
		Potential destruction of a spawning ground	3	1	3	9 (low)
	Construction of a bridge over the river	Restoration of the free flow of water	2	1	3	6 (low)
		Restoration of the free passage of fish and boats	3	1	3	9 (medium)

The evaluation of the anticipated impacts of the dismantling of the culverts and the construction of a bridge over Lake Thomelet River reveals that **negative and positive impacts, on the whole, have the same significance**.

Given those considerations and the fact that restoration of the structure entails negative impacts similar to those associated with the construction of a bridge, **the decision to build a bridge over Lake Thomelet River emerges as the best solution**.

11.4 Question 12: Given the fish species present in the region and the probability that they may be present in the watercourses crossed by the planned road, the project proponent will indicate the periods of the year during which it does not intend to carry out construction work in the water.

Based on the information gathered during consultations with tallymen, the following fish species are present in the watercourses crossed by Barrette-Chapais Itée's planned road. Table 11.4 sums up all the species mentioned by tallymen during private consultations held in the fall of 2009.

Table 11.4: Summary of information regarding fish species present, according to tallymen, along the corridor of the extension to road L-209-N by Barrette-Chapais Itée.

Species	Habitats	Spawning season	Photos
White sucker	Canadian rivers and lakes Found on the entire continental territory of Canada	At the end of winter, it swims upstream in rivers and streams in order to spawn.	
Yellow walleye	Canadian rivers and lakes Found on the entire continental territory of Canada	At the end of winter, it swims upstream in rivers and streams in order to spawn.	
Sauger	Canadian rivers and lakes Found on the entire continental territory of Canada	At the end of winter, it swims upstream in rivers and streams in order to spawn. Spawning season: similar to yellow walleye	
Lake sturgeon	Mainly found in the Hudson and James Bay watershed. Also found in the St. Lawrence River.	End of winter, between May and June. Shallow waters and rapid currents.	

The information obtained regarding species present in the area of the Barrette-Chapais Itée's planned road indicates that all these species spawn in the spring. Hence, to minimize disturbances to these species during their reproductive season, **it is recommended that no bridge construction work be performed from May to the end of June.**

Barrette-Chapais Itée will therefore be able to perform bridge construction work during low water season, in the fall and in the winter. However, bridge construction work shall preferably be performed in the summer and in the fall.

11.5 Question 13: Will some culverts effectively be installed during the winter?

The road construction work is subject to the project being approved by the MDDEP. Furthermore, as explained above, road construction work will qualify for some government of Quebec credits. However, the credit program ends at the end of 2010 and the road construction work must be completed before the end of the program in order to qualify for these credits. Currently, rumour has it that the program might be renewed indefinitely.

The construction of the road will likely take at least 12 months. Consequently, the schedule will extend beyond the end of the above mentioned program and will not qualify for full credits in accordance with the said program.

Once the required authorizations are issued, construction will therefore begin in earnest with the objective of finishing before the end of 2010. **It is therefore very likely that culverts will be installed during the winter. However, it is impossible at this stage to specify which culverts will in fact be installed during the winter.**

11.6 Question 14: With respect to winter road construction work, the project proponent will identify which water crossings will make use of ice-bridges.

Under no circumstances will ice-bridges be built. This work method is not a common practice within the industry. Barrette-Chapais Ltée never uses this approach to cross a watercourse.

In general, when the construction of a road entails crossing a watercourse, the company's ISO 14001 Environmental Management System specifies several procedures that must be followed.

Barrette-Chapais Ltée employs the following methods whenever its equipment must cross a watercourse. The following points illustrate the company's procedures.

Excerpts from Barrette-Chapais Ltée's Environmental Management System:

- Machinery may be brought across a watercourse in order to install a culvert, but the work must be planned so as to minimize the number of crossings (maximum: 1-2 crossings).
- Machinery must never be cleaned less than 60 m from an intermittent or a permanent watercourse.
- Any oil residue generated during cleaning of machinery must be recovered.
- Traveling in the watercourse for the purpose of installing a culvert is permitted, but the operator must ensure that the immersed portion of the machinery is free of any oily residue. The number of excursions into the water must be kept to a minimum (1-2 excursions).

12 Archaeological considerations

- 12.1 Question 17: The project proponent will submit a report from an archaeologist regarding the archaeological potential of the logging road corridor and the locations of the borrow pits.

A report signed by an archaeologist will soon be submitted to the MDDEP (REVCOM). Such report will identify the archaeological potential of the areas affected by the project. The report should list locations within the local assessment area, i.e. within 500 metres on either side of the road's alignment, for both the main and the alternate corridors. This report will also deal with the issue of the locations of borrow pits, and their archaeological potential.

This report is expected to be signed by David Denton, archaeologist for the Cree Regional Authority.

13Forest camp

The initial impact assessment that was submitted – entitled “*Étude d’impact social, économique et sur l’environnement de la région de Chapais et Chibougamau : Construction d’un chemin forestier de transport de bois pour camion hors norme*” – specified that a temporary forest camp would be built in order to house the workers assigned to the construction of the planned road.

Since then, Chantier Chibougamau and Barrette-Chapais Itée have reached an agreement over housing road construction workers at Chantier Chibougamau's BroadBack River facility. Hence, it has therefore been agreed that workers participating in the construction of the planned road will be housed at this facility.

Given this agreement, questions 23-26 no longer require answers.

- 13.1 Question 23: The project proponent will submit information regarding the current state of the site where it plans to set a temporary camp for its workers. It will also describe any work required to prepare the site (e.g. deforestation, earthwork...). The project proponent will describe any facilities (e.g. well, waste water treatment system, waste disposal site...) from the old camp that might be reused.
- 13.2 Question 24: According to Map 5•2 (Campsite) and Map 7•9 (Land use map), the temporary workers' camp appears to be located within the boundaries of the Assinica Wildlife Reserve. If so, has the project proponent obtained all the required authorizations and agreements from the authorities in charge of this wildlife reserve?
- 13.3 Question 25: The project proponent will indicate, on a revised construction schedule, the periods of the year during which the temporary workers' camp will be occupied.
- 13.4 Question 26: Given that waste management is subject to Chapter 22 of the James Bay and Northern Quebec Agreement (Chapter II of the Environment Quality Act), the project proponent will specify how and where it plans to dispose of its waste.

14 Conclusion

The REVCOM's supplementary questions about the planned extension of road L-209-N by Barrette-Chapais Itée provided us with an opportunity to clarify several points that had not yet been explored.

In light of the questions raised by the REVCOM, the impact assessment editorial team believes that a key issue for the REVCOM was to establish which corridor is favoured by Barrette-Chapais Itée for this project.

This interrogation stemmed from the imbroglio caused by the position taken by the area #W12 tallyman (Mr. Simeon Mianscum), namely that the opening up of his family hunting grounds would cause an impact that was irreversible and could not be compensated.

The REVCOM wished that a private consultation be held with tallymen in order to clarify their position regarding the choice between the alternate and the main corridors. Furthermore, this consultation also aimed to verify whether there were any sensitive elements that had not been discussed in the initial document.

This consultation was held in the fall of 2009: it allowed us to gather the information that was missing from the initial document and to clarify our position regarding which of the corridors should be favoured (i.e. the corridor passing west of Lake Ruth or the corridor passing east of this lake).

Recommendations:

In light of the private consultations held with the Departments and the tallymen concerned by the project, the editorial team's recommendations are as follows:

- The main corridor should be selected.
- The presence of the spawning grounds identified by tallymen should be confirmed.
- Measures should be taken to avoid any construction work in water bodies during spawning season wherever and whenever spawning grounds and the fish species involved have been clearly identified.

15 List of references

Samson, C., C. Dussault, R. Courtois and J.-P. Ouellet. 2002. Guide d'aménagement de l'habitat de l'orignal. Société de la faune et des parcs du Québec, Fondation de la faune du Québec et ministère des Ressources naturelles du Québec, Sainte-Foy. 48 pages.

ÉQUIPE DE RÉTABLISSEMENT DU CARIBOU FORESTIER DU QUÉBEC (Woodland caribou recovery team). 2008. *Plan de rétablissement du caribou forestier (*Rangifer tarandus*) au Québec — 2005-2012*. Ministère des Ressources naturelles et de la Faune, Faune Québec, Direction de l'expertise sur la faune et des habitats. 78 pages.

16 Appendices

Appendix 7.1

Report on the meeting with Messrs. Jean Gagnon and Jean-François Lamarre

MDDEP – Direction du patrimoine écologique et des Parcs

Private consultations with the MDDEP – Direction des Parcs regarding the planned extension of road L-209-N

Consultation report

Summary of the meeting held on November 23, 2009 with Messrs. Jean Gagnon and Jean-François Lamarre regarding Barrette-Chapais Itée's plans for the construction of a logging road.

Attendance list:

Jean-François Lamarre : MDDEP	jean-francois.lamarre@mddep.gouv.qc.ca 418-521-3907 x 4494
Jean Gagnon : MDDEP	jean.gagnon@mddep.gouv.qc.ca 418-521-3907 x 4782
Jean-Luc Bugnon : Environnement industriel Bugnon	jean-luc.bugnon@bellnet.ca 418-832-0266

Issues	• Deliberations
Discussion over the boundaries of Assinica Park	<ul style="list-style-type: none">Messrs. Gagnon and Lamarre mention that the boundaries of the future Assinica Park are still the subject of negotiations with the Cree community of Oujé-Bougoumou.The boundaries of the future park are believed to have been modified: a slice being removed on its western side and a corresponding slice being added on the east.A slice measuring several kilometres has been subtracted from the boundaries of the future park, in the area bordering Hydro-Québec's power transmission line (which borders the road under study).Given the sensitivity of the issue, Messrs. Gagnon and Lamarre indicated that it is impossible at this stage to guarantee that the planned road will not fall within the boundaries of the park, and that indeed it is very likely that it does.
Presence of woodland caribou in the Lake Ruth area	<ul style="list-style-type: none">Messrs. Lamarre and Gagnon confirm the presence of a woodland caribou herd near Lake Ruth. This herd has been monitored using tracking collars over a long period of time. This information is said to come from the MRNF – Direction de la Faune.Mr. Lamarre explains that the passage of the road east or west of Lake Ruth will fragment the territory occupied by the woodland caribou.According to Mr. Lamarre, it is very likely that the caribou travel from Lake Ruth to Lake La Trève, and that the road would create a geographical barrier and increase predation in this area.If the boundaries of Assinica Park are as proposed and a slice is in fact subtracted in the west, the MDDEP – Direction des Parks has no further interest in intervening in this file.
Moving the Brock bridge	<ul style="list-style-type: none">Messrs. Gagnon and Lamarre are informed that the Brock Bridge might be moved further downstream in order to protect an existing spawning ground.Mr. Gagnon explains that sensitive areas, where woolly beachheather (<i>Hudsonia tomentosa</i>) can be found, lie further downstream.This plant is a candidate for special status in accordance with the Act respecting threatened or vulnerable species.Brock Bridge is currently located within the boundaries of the future Assinica Park; even if the bridge were moved further downstream, it would still lie within the boundaries of the park.

Appendix 8.6

Report on consultations held in accordance with the MRNF's Land Use
Conflict resolution process

Rencontre de Consultation Meeting

DATE	4 décembre / December 4th, 2008
ENDROIT/PLACE	Val-d'Or
RÉDIGÉ PAR/WRITTEN BY	Hugues Rompré

ÉTAIENT PRÉSENTS/WERE PRESENT:

Eric Labelle, responsable GTC Waswanipi
Jacynthe Barrette, GTC Waswanipi
Hugues Rompré, GTC Waswanipi
Denis Chiasson, Barrette-Chapais
Dario Marceau, Barrette-Chapais
Geoff Quaile, technical advisor
Allan Saganash Jr., Responsible Waswanipi JWG
Simon St-Georges, Coordonnateur MRNF
Isaac Voyageur, Coordonnateur CRIS

ÉTAIENT ABSENTS/WERE ABSENT:

Henry Georges Gull, Waswanipi JWG

COPIE/COPY TO:

Eric Labelle, responsable GTC Waswanipi
Henry Georges Gull, Waswanipi JWG
Jacynthe Barrette, GTC Waswanipi
Hugues Rompré, GTC Waswanipi
Denis Chiasson, Barrette-Chapais
Dario Marceau, Barrette-Chapais
Geoff Quaile, technical advisor
Allan Saganash Jr., Responsible Waswanipi JWG
Simon St-Georges, Coordonnateur MRNF
Isaac Voyageur, Coordonnateur CREE

Version française	English version
À la suite des rencontres de participation du PGAF 2008-2013 de l'UAF 26-65, trois cas seront discutés	Following the participation meetings for the 2008-2013 GFMP of FMU 26-65, three cases will be

<p>avec les coordonnateurs.</p> <p>Cas n°1 - 10 h W-16 / W-21 / W-23 / W-21A PQAF 2008-2013 / Les maîtres de trappes sont présents.</p> <p>Eric Labelle présente la proposition initiale concernant le chemin de Barrette-Chapais qui traverse l'AT W-16. Au PGAF, deux propositions de chemins ont été déposées. L'une d'entre elles provient du maître de trappe et évite le ravage d'original dans son aire de trappe. La compagnie n'est pas en désaccord avec la proposition du trappeur. Une visite terrain devra être effectuée l'été prochain afin d'évaluer la possibilité de réaliser la proposition de chemin au sud de l'aire de trappe.</p> <p>La compagnie veut faire des échanges de bois avec Nabakatuk et utiliser des camions hors normes pour réduire les coûts de transport.</p> <p>Le maître de trappe de l'AT W-16 aimerait connaître les commentaires des autres trappeurs (W-23 et W-21). Le trappeur veut que les deux autres trappeurs soient en accord avec la proposition du chemin avant même de prendre une décision. Le maître de trappe de l'AT W-21 ne voit pas de problème avec la construction du chemin dans son aire de trappe.</p> <p>L'objectif est de limiter le nombre d'accès dans une aire de trappe, mais on ne peut pas arrêter le développement.</p> <p>Le maître de trappe ne semble pas vraiment en accord avec la proposition. Il aimerait qu'aucun bail ne soit émis dans son aire de trappe. Il faudra vérifier auprès du secteur Territoire du MRNF si cela est possible. Il serait étonnant qu'une telle demande soit acceptée.</p> <p>Le maître de trappe a beaucoup de préoccupations avec la coupe dans son aire de trappe. Il parle également du fait qu'il aimerait qu'il y ait uniquement des chemins d'hiver dans son aire de trappe. On lui mentionne cependant que cela n'est pas possible : la plupart du temps, les chemins d'accès sont en gravier. Le maître de trappe mentionne que les ententes prises dans le passé n'ont pas été respectées.</p> <p>Barrette-Chapais indique que les relations avec</p>	<p>discussed with the coordinators.</p> <p>Case no. 1 - 10 am W-16 / W-21 / W-23 / W-21A 2008-2013 FYFMP / Tallymen are present.</p> <p>Eric Labelle presents the initial proposal concerning Barrette-Chapais' road cutting across trapline W-16. Two road proposals were submitted under the GFMP. One of them originates from the tallyman. The road he proposes would not run through the moose yard in his trapline. The company does not disagree with the trapper's proposal. A field visit will be required next summer to assess the possibility of constructing a road in the southern part of the trapline.</p> <p>The company wants to trade timber with Nabakatuk and use oversized trucks to reduce hauling costs.</p> <p>The tallyman of trapline W-16 would like to get the comments of the other trappers concerned (W-23 and W-21). The trapper wants the other two trappers agree with the road proposal first before taking any decision. The tallyman of trapline W-21 has no problem with the construction of the road in his trapline.</p> <p>The objective consists in restricting access roads in a trapline. However development cannot be stopped.</p> <p>The tallyman does not seem to agree fully with the proposal. He would prefer not see any lease delivered in his trapline. We must check with MRNF Territory if that is possible. Acceptance of this request would be surprising.</p> <p>The tallyman is very much concerned about felling operations in his trapline. He also mentions that he would prefer no road in his trapline other than winter roads. He is told that this is not possible, for in most cases access roads are graveled. The tallyman mentions that past agreements have not been respected.</p>
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<p>Mishtuk sont encore bonnes. Les deux parties veulent continuer les affaires. Selon Allan Saganash jr., les négociations entre les deux compagnies seraient finies et aucune entente n'aurait été conclue. Nous devrons cependant attendre le résultat officiel des négociations.</p> <p>Le trappeur voudrait que le chemin passe dans l'aire de trappe W-21A et ainsi partager l'impact avec les autres trappeurs.</p> <p>La partie crie pense qu'en déplaçant le chemin dans l'aire de trappe W-21A, la compagnie pourrait récupérer le bois dans la partie sud de l'AT W-21A et limiter l'accès à l'AT W-23.</p> <p>Nous devrons en premier lieu discuter de cette option avec Sam C. Gull (W21A) et recevoir ses commentaires. Cependant, il y a déjà un accès dans l'AT W-23, pour récupérer le bois dans l'AT W-23 et la compagnie ne pense pas accéder à la partie sud de l'AT via ce nouvel accès.</p> <p>Nous devrons rencontrer Sam C. Gull afin de l'informer du nouveau chemin dans son aire de trappe et obtenir ses commentaires. Toutes ces options devront être analysées sur le terrain.</p> <p>Selon la compagnie, il ne semble pas logique de construire des chemins pour accéder à la partie sud de l'aire de trappe W-21A à partir du nord-est. Geoff Quaile mentionne qu'il y a déjà un accès dans la partie sud-est; la compagnie pourrait donc sortir le bois par un accès seulement. Les Cris ont beaucoup de problèmes avec les nouveaux accès : camions hors norme, nouveaux baux, augmentation de la pression de chasse...</p> <p>Une nouvelle rencontre sera nécessaire afin de rencontrer à nouveau les trappeurs en présence des coordonnateurs. La rencontre sera à Waswanipi le 15 janvier 2009.</p> <p>Cas n° 2 - 14 h W-10 / W-10A / W-12 / W-22 PQAF 2008-2013</p> <p>Ce cas concerne deux propositions de chemins de Barrette-Chapais afin d'accéder au nord de l'UAF 26-65. Pour l'instant, il y a deux accès étant donné que le statut du parc Assinica n'est pas encore</p>	<p>Barrette-Chapais indicates that relationships with Mishtuk are still good. Both wish to do business together. According to Allan Saganash jr., the negotiations between the two have come to an end and no agreement has been reached. We must wait and watch the outcome of the negotiations.</p> <p>The trapper would like the road to run through trapline W-21A and share the impact with other trappers.</p> <p>The Cree party believes that once the road is relocated in trapline W-21A the company would be able to get timber out of the southern part of trapline W-21A and limit access to trapline W-23.</p> <p>First we must discuss this option with Sam C. Gull (W21A) and wait for his comments. However there already is an access road in trapline W-23 to get timber out of trapline AT W-23 and the company does not plan to access the southern part of trapline via this new access road.</p> <p>We must meet with Sam C. Gull to advise him about the new road in his trapline and get his comments. All of those options must be analyzed during a field visit.</p> <p>According to the company, constructing roads to access the southern part of trapline W-21A via northeast does not seem logical. Geoff Quaile mentions there exists an access road in the southeastern part of the trapline already. The company could therefore haul the timber out via one access road only. The Crees are quite reluctant to the idea of constructing additional access roads: oversized trucks, new leases, increase in hunting pressure ...</p> <p>It is necessary to schedule another meeting with the trappers and the coordinators. The meeting will be held in Waswanipi on January 15, 2009.</p> <p>Case no. 2 - 2 pm W-10 / W-10A / W-12 / W-22 2008-2013 FYFMP</p> <p>This case concerns Barrette-Chapais and road proposals to access FMU 26-65 via north. For the</p>
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connu. Idéalement, la proposition du chemin qui passe par le nord serait privilégiée. Un seul des chemins proposés sera construit.

Selon Simon St-Georges, il y aurait peut-être une possibilité de faire passer le chemin dans le parc Assinica si le MRNF en fait la recommandation au MDDEP. Pour l'instant, nous allons continuer de considérer les deux options.

La compagnie mentionne qu'Hydro-Québec ne veut pas que les compagnies forestières se servent de la route sous les pylônes pour une question de sécurité et de protection de leurs équipements et qu'il serait impensable de l'élargir. Il y a de gros fils de fer qui descendent tout près de la route pour soutenir les pylônes. De plus, la compagnie veut utiliser des camions hors normes pour transporter le bois.

La compagnie devra demander un avis technique à Hydro-Québec concernant l'agrandissement de la route.

La partie crie propose un autre accès qui se voudrait une boucle dans l'AT W10A afin qu'il n'y ait pas de connexion avec l'AT W10. À partir du début de l'AT W-10A, la compagnie pourrait construire un autre chemin près de la ligne d'Hydro-Québec ou bien élargir celui déjà en place.

En plus, il y aura plus de baux qui seront émis, augmentant ainsi la pression de la chasse.

Il y a déjà une planification quinquennale dans les aires de trappe W-10 et W-10A. Il faudrait donc porter une attention à ce qu'il n'y ait pas de connexion entre la boucle et les chemins existants. Avec la boucle, il serait possible d'exploiter la partie sud de l'AT W-10A. La compagnie regardera la faisabilité de construire un chemin ou d'élargir la route d'Hydro-Québec à partir de l'AT W-10A.

Simon St-Georges propose de fermer l'accès de la route du Nord à partir de l'AT W-10, mais Allan Saganash mentionne que la pression de chasse ne viendrait pas de la route du Nord, mais plutôt du nouvel accès. Les trappeurs d'Oujé-Bougoumou sont en accord avec la proposition des chemins.

Barrette-Chapais préférerait garder sa proposition et fermer l'accès de la route du Nord. La partie crie

time being, there are two access roads because the status of Assinica Park is still unknown. Ideally preference should be given to the road via north. Only one road will be constructed.

According to Simon St-Georges, it may be possible to have the road cut across Assinica Park if the MRNF recommends this alternative to the MDDEP, but for the time being we will continue to consider the two options.

The company mentions that Hydro-Québec refuses to let the timber companies use the road running along the transmission towers for safety and equipment protection reasons. Widening the road is unthinkable. There are thick iron cables anchored into the ground near the roadside to support the towers, and the company wants to use oversized trucks.

The company will have to seek technical advice from Hydro-Québec concerning the widening of the road.

The Cree party suggests another access road – a loop in trapline W10A – that would not connect with trapline W10. The company would have to build another road near HQ transmission line from the limits of trapline W-10A or to widen the existing road.

Besides there will be a greater number of leases delivered, thus causing increase in hunting pressure.

There is a five-year plan for traplines W-10 and W-10A. We must make sure that the loop does not connect with existing roads. The loop will make it possible to harvest the southern part of trapline W-10A. The company will check the possibility to construct a road or widen HQ's road from trapline W-10A.

Simon St-Georges suggests closing access to route du Nord from trapline W-10, but Allan Saganash mentions that hunting pressure would not come from route du Nord, but from the new access road.

<p>maintient sa position et aimerait que la compagnie construise un chemin parallèle à la route du Nord.</p> <p>Le trappeur nous informe de la présence d'une frayère là où il est prévu de faire passer le chemin et il ne veut pas non plus que le chemin passe dans son 1 %. À cet endroit sa famille possède 6 ou 7 camps.</p> <p>On devra se revoir à nouveau pour discuter des propositions. Dans l'entre-temps, la compagnie pourra analyser les propositions de la partie crie. La rencontre sera également le 15 janvier à Waswanipi.</p> <p>Cas n° 3 Trappeur absent W-21A (Accès de W-23 et W-16) PQAF 2008-2013 (voir cas n° 1)</p> <p>Des discussions en lien avec ce cas se sont produites lors de la rencontre avec le cas n° 1. Étant donné que le maître de trappe est absent, nous devrons discuter à nouveau de ce cas le 15 janvier 2009.</p>	<p>The trappers of Oujé-Bougoumou concur with the road proposals.</p> <p>Barrette-Chapais would prefer to see its proposal implemented and access from route du Nord closed. The Cree party maintains its position and would like the company to build a road parallel to route du Nord.</p> <p>The trapper informs us that the road as planned would cut across a spawning area. The trapper also objects to see the road cut across his 1%, where his family has 6 or 7 camps.</p> <p>We must schedule another meeting to discuss the proposal. In the meantime, the company can study the Cree proposals. The meeting will also be held in Waswanipi on January 15.</p> <p>Case no. 3 – Trapper is absent W-21A (Access road W-23 & W-16) 2008-2013 FYFMP (refer to case no. 1)</p> <p>Discussions concerning this case were held during meeting to discuss case no. 1. Since the trapper is absent, this matter must be discussed on January 15, 2009.</p>
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**Ministère des
Ressources naturelles
et de la Faune**



Direction régionale du Nord-du-Québec

Rencontre de Consultation Meeting

DATE	15 janvier / January 2009
ENDROIT/PLACE	Waswanipi
RÉDIGÉ PAR/WRITTEN BY	Hugues Rompré

ÉTAIENT PRÉSENTS/WERE PRESENT:

Jacynthe Barrette, GTC Waswanipi
Hugues Rompré, GTC Waswanipi
Denis Chiasson, Barrette-Chapais
Geoff Quaile, Technical Advisor
Allan Saganash Jr., Responsible Waswanipi JWG
Simon St-Georges, Coordonnateur MRNF
Isaac Voyageur, CREE Coordinator
Véronique Martel, MNRF

ÉTAIENT ABSENTS/WERE ABSENT:

Eric Labelle, responsable GTC Waswanipi

COPIE/COPY TO:

Eric Labelle, responsable GTC Waswanipi
Henry Georges Gull, Waswanipi JWG
Jacynthe Barrette, GTC Waswanipi
Hugues Rompré, GTC Waswanipi
Denis Chiasson, Barrette-Chapais
Dario Marceau, Barrette-Chapais
Geoff Quaile, Technical Advisor
Allan Saganash Jr., Responsible Waswanipi JWG
Simon St-Georges, Coordonnateur MRNF
Isaac Voyageur, CREE Coordinator

Version française	English version
<p>Cette rencontre découle des trois cas de différends relatifs à l'utilisation du territoire dans l'UAF 26-65. Une première rencontre a eu lieu le 4 décembre 2008, à Val-d'Or. L'objectif de la rencontre d'aujourd'hui est de poursuivre les discussions amorcées en décembre dernier en vue de résoudre les différends.</p>	<p>This meeting results from three land-use conflicts in FMU 26-65. A first meeting was held on December 4, 2008, in Val-d'Or. The purpose of today's meeting is to continue discussions initiated in December in order to solve the conflicts.</p>
<p>Cas n° 1 - 10 h 50 W-10 / W-10A / W-12 / W-22 PQAF 2008-2013 – Maîtres de trappe présents</p>	<p>Case no. 1 – 10:50 am W-10 / W-10A / W-12 / W-22 2008-2013 FYFMP – Tallyment are present</p>
<p>Allan Saganash débute la rencontre dans la langue crie pour expliquer les nouvelles propositions de Barrette-Chapais concernant la construction de chemins aux maîtres de trappe. Plusieurs propositions ont été élaborées puisque le périmètre final du parc Assinica n'est pas encore officiel.</p>	<p>Allan Saganash starts the meeting in Cree language to explain Barrette-Chapais' new road proposals to the tallymen. Several proposals have been developed, since the final boundaries of Assinica Park are not yet official.</p>
<p>Après avoir pris connaissance des propositions, les trappeurs de l'aire de trappe W10 (Georges et son père) aimeraient que Barrette-Chapais utilise une partie du chemin déjà présent dans leur aire de trappe. La compagnie devra étudier cette option, mais à première vue, cela semble possible.</p>	<p>After looking into the proposals, the trappers of trappleine W10 (Georges and son) would like Barrette-Chapais to use part of an existing road in their trappleine. The company will have to study that alternative, but at first glance their proposal seems possible.</p>
<p>En ce qui a trait du chemin (boucle) que la compagnie prévoit construire dans l'aire de trappe W10A et à la suite de la demande de la partie crie d'éviter l'interconnexion avec l'aire de trappe W10, le maître de trappe exprime certaines préoccupations concernant le chemin qui passe dans son 25 %. Il demande s'il sera possible de déplacer son 25 % ou le chemin que la compagnie prévoit construire. La compagnie mentionne que ce cas pourrait être discuté au plan annuel et qu'une analyse plus détaillée devra être effectuée avant que la compagnie prenne position. Le site d'intérêt faunique dans ce secteur revêt une très grande importance pour le maître de trappe. Le fait qu'il n'y ait pas de connexion entre les aires de trappe W-10 et W-10A constitue un enjeu important.</p>	<p>With regard to the road (loop) the company plans to build in trappleine W10A and pursuant to the Cree party's request to avoid any connection with trappleine W10, the tallyman expresses some concerns about the road that runs in his 25%. He asks whether it would be possible to relocate either his 25% or the road the company plans to build. The company mentions that this case can be discussed within the framework of the annual plan and that a more detailed analysis must be completed before the company can take a position. The site of wildlife interest in that area is of great importance to the tallyman. Not having a connection between trappleines W-10 and W-10A is an important issue.</p>
<p>Allan Saganash mentionne que le chemin dans ce secteur est situé sur des terres de catégorie II. Cela dit, la partie crie n'est pas intéressée à ce qu'un chemin soit construit sur des terres de catégorie II tant et aussi longtemps qu'il n'y aura pas de blocs de coupes de planifiés. La partie crie aimerait que ce chemin arrête avant la limite des terres de catégorie II afin d'éviter qu'on y exerce une pression de chasse et pêche inutile. Barrette-Chapais ne voit</p>	<p>Allan Saganash mentions that the road in that sector is located on Category II lands. This being said the Cree party is not interested in seeing a road built on Category II lands until planning includes cut blocks in the area. The Cree party would like the road to end before the limit of Category II lands to avoid unnecessary fishing and</p>

<p>pas de problème à ce que le chemin s'arrête avant la limite des terres de catégorie II.</p>	<p>hunting pressure in the area. Barrette-Chapais has no problem with stopping the road before the limit of Category II lands.</p>
<p>La route d'accès de Barrette-Chapais passe à l'intérieur de deux 1 %. Le premier se situe dans l'aire de trappe W-10A. Dans ce cas, le maître de trappe a donné son accord à ce que le chemin passe dans son site d'intérêt particulier (1 %). Le deuxième est situé dans l'aire de trappe W-22. Cependant, il ne s'agit pas du chemin que la compagnie priorise. Le maître de trappe n'assiste pas à la rencontre.</p>	<p>Barrette-Chapais' access road runs inside two areas of particular interest (1%). The first area is located in trapline W-10A, where the tallyman agrees to let the road run inside his 1%. The second is located in trapline W-22. This road is not the one prioritized by the company. The tallyman does not attend the meeting.</p>
<p>Pour ce qui est de l'aire de trappe M-47A, le GTC de Mistissini devra prévoir une rencontre avec le maître de trappe afin de le consulter sur le nouveau chemin dans son aire de trappe.</p>	<p>With regard to trapline M-47A, the Mistissini JWG must schedule a meeting with the tallyman so as to consult him on the new road in his trapline.</p>
<p>En résumé, les maîtres de trappe des aires de trappe W-10, W-10A et W-12 sont en accord avec les propositions de Barrette-Chapais concernant la construction de chemins qui leur sont présentés.</p>	<p>In a nutshell, the tallyman of traplines W-10, W-10A and W-12 agree with Barrette-Chapais' road proposals presented to them.</p>
<p>Cas n° 2 - 13 h 35 W-21A (Accès de W-23 et W-16) PQAF 2008-2013 Trappeur présent</p>	<p>Case no. 2 – 1: 35 pm W-21A (Access to W-23 and W-16) 2008-2013 FYFMP Trapper is present</p>
<p>Sam C.Gull assiste à la rencontre. Il voudrait que le bois soit récolté via les chemins d'accès en provenance du nord ou de l'ouest de son aire de trappe. Le chemin que la compagnie souhaite construire dans l'aire de trappe W-16 n'a aucun lien avec la récolte à effectuer dans la partie est de son aire de trappe.</p>	<p>Sam C. Gull attends the meeting. He wants all timber harvested via access roads coming from north or west. The road the company would like to build in trapline W-16 has no link whatsoever with harvesting operations to be carried out in the eastern part of his trapline.</p>
<p>La compagnie mentionne qu'il existe déjà un chemin dans l'aire de trappe W23. Construit par Kruger, il permet un accès à l'aire de trappe W-21A.</p>	<p>The company mentions that there is a road in trapline W23. Built by Kruger, the road gives access to trapline W-21A.</p>
<p>La compagnie pourrait passer par trois chemins existants pour transporter le bois via l'aire de trappe W-23, mais le maître de trappe mentionne de nouveau qu'il aimerait que la compagnie accède à son aire de trappe via le nord ou l'ouest de son aire de trappe pour récupérer le bois dans ces secteurs.</p>	<p>The company could use three existing roads to haul timber via trapline W-23, but the tallyman reiterates that he would like to see the company get to his trapline via north or west of his trapline to take the timber out of those areas.</p>
<p>La partie crie veut limiter le nombre d'interconnexions avec l'aire de trappe W-23. Dans ce cas, il faudrait utiliser un seul chemin pour accéder à ces secteurs (récupération et transport du bois).</p>	<p>The Cree party wants to limit interconnections with trapline W-23. Therefore it would have access to those areas via one road only (wood salvage and haulage).</p>
<p>Après analyse de la situation, la compagnie est en</p>	

<p>accord avec cette demande et utilisera seulement un chemin pour brancher l'aire de trappe W-23 à l'aire de trappe W-21A (chemin le plus au sud). La compagnie s'engage également à réparer un chemin près du lac Lapparent. Le maître de trappe est également d'accord pour que la réfection soit effectuée dans le 1 % près du lac Lapparent.</p> <p>De cette manière, le différend serait réglé. Toutes les parties sont en accord avec ces nouvelles propositions.</p> <p>Cas n° 3 W-16 / W-21 / W-23 / W-21A PQAF 2008-2013 – Trappeur est absent</p> <p>La construction de ce chemin vise avant tout de permettre l'échange de bois avec Nabakatuk. Sam C. Gull semblait en désaccord avec le fait que le chemin passe dans son aire de trappe pour absorber une partie des impacts. Ceci créerait une interconnexion qui n'est pas nécessaire dans cette situation.</p> <p>Finalement, Sam C. Gull accepte que le chemin passe en partie dans son aire de trappe pour éviter le 25 % dans l'aire de trappe W16. La longueur totale du chemin correspond à environ 20 km en classe 1 et 15 km en classe 3. Le nouveau chemin passerait en partie dans l'ancien chemin de fer pour éviter la construction d'un nouveau chemin.</p> <p>Étant donné que Jimmy et le maître de trappe sont absents, nous avons présenté la planification à la partie crie, qui se chargera de consulter le maître de trappe et de recueillir ses commentaires.</p> <p>Fin de la rencontre : 14 h 40</p>	<p>After studying the situation, the company agrees with the request and will use only one road to connect trapline W-23 and trapline W-21A (southernmost road). The company commits to repair a road near lac Lapparent. The tallyman also agrees to let a road repaired in the 1% near lac Lapparent.</p> <p>This would solve the conflict. All parties agree with the new proposals.</p> <p>Case no. 3 W-16 / W-21 / W-23 / W-21A 2008-2013 FYFMP – Trapper is absent</p> <p>The construction of this road aims firstly to make timber exchange with Nabakatuk possible. Sam C. Gull seemed to disagree with the road passing through his trapline to absorb part of the impact. That would result in creating an interconnection that is unnecessary in the circumstance.</p> <p>Finally, Sam C. Gull accepts to let part of the road run in his trapline to avoid the 25% in trapline W16. The road will total about 20 km in length (class 1) and 15 km (class 3). Part of the new road will follow a former railroad to avoid building a new road.</p> <p>Since Jimmy and the tallymen are absent, the planning was presented to the Cree party, who in turn will consult the tallyman and get his comments.</p> <p>End of meeting: 2:40 pm</p>
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Appendix 9.1

Report on the private consultations held with tallymen affected by the planned extension of road L-209-N by Barrette-Chapais Itée.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : December 14, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : W10
Person Concerned with this trapline : Joseph Neeposh
Persons present during the private Consultation :
George Neeposh, son of tallyman for trapline W10
Judy Trapper, Waswanipi JWG coordinator
Eric Labelle, MRNF
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
Presentation of the Project A specific map of the Trapline affected by this project was presented. This map shows the proposed layout, as well as all Company information. Not only important points will be presented, but also any incomplete or missing information.

Specific questions
<p>Q1 : What road seems to be better – the layout going west or east of Ruth Lake ?</p> <p>George Neeposh says that he prefers the East side.</p>
<p>Q1.1 : For what reason ?</p> <p>Because there is already a road on the East side, and on the West side, there is a river to cross and possible a spawning area.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>There is no way to prevent an impact on the West side, but when the road will be built, sedimentation in the river will affect the quality of the water for the fish.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>George Neeposh would like Barrette-Chapais to repair the access road leading to his father's camp (#39 on the map) (agreement request already registered at GFMP). And, if there works done this winter, he asks that this road and the access road to his camp (#40 on the map) be snowplowed at least once in January and once in March before « GooseBreak ».</p> <p>Also he would like signs to be put up to inform everyone that this territory is a Category II Land.</p> <p>He also asks to preserve access to a spring (#42 on the map) (agreement request already registered at GFMP).</p> <p>If there will be forestry harvesting he asks to leave some birch from time to time on the side of the roads for firewood.</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>George Neeposh tells of many spawning areas that are situated at the very end of the road layout (#43 on the map). The others are not close to the road project. He does not want to reveal the species.</p> <p>He also points out a sector used for caribou (#44 on the map).</p>
<p>Q5 : Is there an ancestral site in the cprridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>George Neeposh identifies a burial ground at Caminscanane Lake (#45 on the map).</p>
<p>Q6 : May we use your actual or traditional knowledge in the present study of this</p>

impact ?

Yes.

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

Yes, to show the spawning area (#43 on the map) but he wants to keep the species found there confidential.

Q6.2 : If not, why would you refuse to give us this information ?

N/A

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

George Neeposh points out a snowmobile trail (#46 on the map) and a portage (#47 on the map) as well as two camps (#48 and #49 on the map).

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

George Neeposh says that he wants his territory to stay intact as much as possible especially to conserve the wildlife around there. He says that he has everything he needs to live on his land.

Q9 : On the whole, do you agree with this project ?

He is not in favour of the forest harvesting in general, but doesn't really have a choice. But he understands this is for economic reasons and creates employment.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : December 14, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : W10A
Person Concerned with this trapline : Johnny Trapper
Persons present during the private Consultation :
Johnny Trapper, tallyman for trapline W10A
Albert Trapper, son of tallyman for trapline W10
Jimmy Neeposh, trapline W10A user
Judy Trapper, Waswanipi JWG coordinator
Eric Labelle, MRNF
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented. This map shows the proposed layout, as well as all Company information. Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What road seems to be better – the layout going west or east of Ruth Lake ?</p> <p>The tallyman prefers the West side of Ruth Lake.</p>
<p>Q1.1 : For what reason ?</p> <p>Because the East side of Ruth Lake is Oujebougoumou territory and he wish to continue trapping Marten along the road. However, he agrees that the road passes on the East side if trapper Simeon Mianscum is opposed to the West side.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>Don't build the road too close to the lakes, about 200 meters would be fine (#10 on the map).</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Johnny Trapper asks if it would be possible to build an access road to the Omo Lake (#21 on the map) And also repairs the existing road (#22 on the map).</p> <p>Johnny Trapper also asks if it would be possible that Barrette-Chapais Ltée open the old Hydro-Québec dump for his garbage (#23 on the map).</p> <p>Johnny Trapper also wants Barrette-Chapais Ltée to put gravel on the existing road at km 54 that has access to Omo Lake (#24 on the map). He asks that Barrette-Chapais Ltée plows the snow from this road if they are working close by.</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>Johnny Trapper says that he regularly sees caribou tracks on the Hydro-Quebec road (#25 on the map). And he also shows us the sectors where the caribou stay around in the spring (#26 on the map) and during winter (#27 on the map).</p> <p>He shows us two spawning areas but wants to keep the species confidential (#28 & #29 on the map).</p> <p>He does not see any other animals in danger.</p>
<p>Q5 : Is there an ancestral site in the cprridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>Johnny Trapper says that there are no burial grounds in this area.</p>

Q6 : May we use your actual or traditional knowledge in the present study of this impact ?

Yes.

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

Yes, to show the spawning area (#28 & #29 on the map) but not the species.

Q6.2 : If not, why would you refuse to give us this information ?

He wishes this to remain confidential.

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

Johnny Trapper points out his main camp site, (#30 on the map) and old Hydro-Quebec dump that he wants to have opened for his waste (#23 on the map) and two snowmobile trails (#31 and #32 on the map).

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

Johnny Trapper wants to know when the road construction will begin. We tell him that it depends on when we get the authorization, but we hope to start working there next spring.

He asks if a forestry camp will be installed during the road construction. We inform him that Chantiers Chibougamau's Broadback camp will be used during construction.

He would like the road to be plowed during the spring before the « Goose Break ».

He also asks if we expect to have parking area on the side of the road during logging. We inform him that at least one parking area is expected every two kilometers.

He tells us that he wants to hunt beaver in the autumn before demolishing the beaver dams for road construction.

He wants to know if he could use the same radio frequency used by the loggers.

Q9 : On the whole, do you agree with this project ?

Johnny Trapper agrees with the project and says it will help him to get to his trapline

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : November 20, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : M47A
Person Concerned with this trapline : Charlie John Coon
Persons present during the private Consultation :
Charlie John Coon, tallyman for trapline M47A
Johnny Coon, son of tallyman for trapline M47A
Blazo Voyageur, JWG coordinator from Mistissini
John-Henry Shecapio, JWG coordinator from Mistissini
Valérie Guindon, MRNF
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented.</p> <p>This map shows the proposed layout, as well as all Company information.</p> <p>Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What road seems to be better – the layout going west or east of Ruth Lake ?</p> <p>Charlie John Coon says there is less impact on the east side.</p>
<p>Q1.1 : For what reason ?</p> <p>Because the lakes will be more accessible to more people on the west side.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>None.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Charlie John Coon asks that Barrette-Chapais Ltée build an access road (#15 on the map) at Lake Capiehigamau and move his camp there (#16 on the map), (in accordance with the GFMP understanding).</p> <p>Charlie John Coon also wants to know if it is possible for Barrette-Chapais Ltée to install a gate at the entrance of this access road to Lake Capiehigamau (#19 on the map) in order to protect his property.</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>Charlie John Coon points out a spawning area (#17 on the map) but doesn't want to mention the species there. This spawning area is at a rapid, very close to the culverts planned for the road that is under study.</p> <p>He also mentioned that moose will be affected during construction of the road.</p>
<p>Q5 : Is there an ancestral site in the cprridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>Charlie John Coon paints out on the maps an old camp site situated between 2 lakes (#18 on the map), and asks that no access roads be built going towards these 2 lakes.</p>
<p>Q6 : May we use your actual or traditional knowledge in the present study of this impact ?</p> <p>Yes.</p>

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

Yes, to show the spawning area (#17 on the map)

Yes, to show the old camp site (#18 on the map)

Q6.2 : If not, why would you refuse to give us this information ?

N/A

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

The layout of the road passes inside of a particular site of interest (1%) Charlie John Coon would like this site placed elsewhere. (Blazo Voyageur will ask the Cree Regional Administration if is possible to replace the site as asked by the Tallyman). If this is not possible, Charlie John Coon asked to move the road a bit to avoid this site. (For reasons of confidentiality this 1% site will not appear on the map).

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

Charlie John Coon says that at this present time, Lake Capiehigamau (#20 on the map) is not contaminated and he hopes that construction of this road doesn't deteriorate the quality of the water.

Charlie John Coon suggested that a gate be installed at the entrance of the road to control commings and goings. This will permit us to know who circulates on the premises and to easily identify wrongdoers and specially for security reasons.

Q9 : On the whole, do you agree with this project ?

Charlie John Coon says that even though he agrees with this project of the road, this will give access to too many people to this territory.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : December 14, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : W12
Person Concerned with this trapline : Simeon Mianscum
Persons present during the private Consultation :
Simeon Mianscum, tallyman for trapline W12
Judy Trapper, Waswanipi JWG coordinator
Eric Labelle, MRNF
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented.</p> <p>This map shows the proposed layout, as well as all Company information.</p> <p>Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What road seems to be better – the layout going west or east of Ruth Lake ?</p> <p>Simeon Mianscum says that he prefers the West side of Ruth Lake.</p>
<p>Q1.1 : For what reason ?</p> <p>Because he thinks that if he asks for the West side, we will build the road on the East side and vice versa.</p> <p>He mentions that if the road is built on the West side, it will affect the quality of the water, increase the number of fisherman on Lake Caupichigau, and will give access to his territory. According to him, the preference of caribou east of Ruth Lake doesn't justify building the road on his hunting ground.</p> <p>He also mentions that he doesn't hunt in this sector. However, a road built west of Ruth Lake will permit him to built a camp close to Lake Caupichigau.</p> <p>After thinking about it, he says that he prefers the road built west of Ruth Lake instead of east. This will open his territory but will also allow him to built a camp close to the lake we are discussing.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>He says that the impact on his territory will be irreversible and nothing will make up for it in any way.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Simeon Mianscum says for many years that he asked that the access road to his camp (#37 & #38 on the map) be repaired. His camp and the access road are not close to the road project.</p> <p>He also says that if the road will be built, he would like to have a site to built a camp at Caupichigau Lake (#39 on the map).</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>Simeon Mianscum points out a spawning area for suckers, but wants to keep the information confidential.</p>

Q5 : Is there an ancestral site in the corridor of the proposed road ? If yes, would you please let us know where it is ?

Simeon Mianscum says that his father had a camp at Lake Caupichigau many years ago.

He also mentions a burial ground near by.

This burial sites are not close to the road project.

Q6 : May we use your actual or traditional knowledge in the present study of this impact ?

He wishes to keep the information confidential.

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

N/A.

Q6.2 : If not, why would you refuse to give us this information ?

N/A.

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

Simeon Mianscum says that there are none in that sector.

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

He thinks that if the road is built, the quality of the water will be affected on Lake Caupichigau.

He also mentions that many animals are affected by work in the winter because their habitats are destroyed by the machinery.

Q9 : On the whole, do you agree with this project ?

Simeon Mianscum says that if it's not possible to do otherwise for the construction of this road, he will not against it.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : December 14, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : W22
Person Concerned with this trapline : Willy Wapachee
Persons present during the private Consultation :
Willy Wapachee, tallyman for trapline W22
Judy Trapper, Waswanipi JWG coordinator
Eric Labelle, MRNF
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented.</p> <p>This map shows the proposed layout, as well as all Company information.</p> <p>Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What road seems to be better – the layout going west or east of Ruth Lake ?</p> <p>The tallyman prefers the West side of Ruth Lake.</p>
<p>Q1.1 : For what reason ?</p> <p>Because it will be easier to visit his friend, Johnny Trapper at Omo Lake.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>No comment.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Willy Wapachee shows us a temporary camp site on the banks of Thomelet River (#33 on the map). He also wishes Barrette-Chapais Ltée to clear out a bigger site so he could install a permanent camp as well as a parking area. He would put up the ribbons to indicate the area.</p> <p>He also wishes that the Hydro-Québec road that joins the Barrette-Chapais Ltée road project be regularly plowed and maintained before the « Goose Break » (#34 on the map).</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>Willy Wapachee points out a walleye spawning area, but he wishes to keep the spot confidential.</p> <p>He says that he sometimes sees caribou tracks on the Hydro-Quebec road, but he says that the caribou are mostly around Lake Latrèvre.</p> <p>There are no other species in danger.</p>
<p>Q5 : Is there an ancestral site in the cprridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>He says there is none near the layout of planned road.</p>
<p>Q6 : May we use your actual or traditional knowledge in the present study of this impact ?</p> <p>Yes, but he prefers not to reveal the walleye spawning area.</p>

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

See map.

Q6.2 : If not, why would you refuse to give us this information ?

He wants to keep this confidential.

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

Willy Wapachee points out the place for his main camp (#36 on the map) and a temporary camp (#33 on the map) that he uses mostly in the spring during the « Goose Break » and summer for fishing.

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

We tell Willy Wapachee that Barrette-Chapais Ltée is planning to built a bridge to cross the Thomelet River (#35 on the map) to replace the existing culverts. He says that it is fine to built a bridge there because it will

Allow him to use his boat.

He says that the new bridge will re-establish normal current that was there before Hydro-Quebec installed the culverts. Also Mr. Wapachee says there was a spawning area a little upstream from the culverts and the bridge could re-established this spawning area.

Q9 : On the whole, do you agree with this project ?

Willy Wapachee says that he has no problem with this project.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : November 20, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : O52 Consultation simultaneously with trapline O53.
Person Concerned with this trapline : David Bosum Sr.
Persons present during the private Consultation :
Sam Bosum, trapline O52 tallyman's brother
Charlie Bosum, tallyman for trapline O53
Walter Bosum, trapline O53 tallyman's son
Tommy RabbitSkin, JWG coordinator from Oujeboougoumou
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented. This map shows the proposed layout, as well as all Company information. Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What route seems to be better – the layout going west or east of Ruth Lake ?</p> <p>Sam Bosum says that to go east of Ruth Lake would be preferable.</p>
<p>Q1.1 : For what reason ?</p> <p>Sam Bosum says that by going west of Ruth Lake would make it accessible to two lakes (#9 & #10 on the map) And it would be easier for more people to get there. Whereas it is already accessible east of Ruth Lake.</p> <p>Also Sam Bosum doesn't think that the caribou will be affected by this proposed route because the caribou could change locations. Furthermore, he says to have travelled many times east of Ruth Lake and has never seen any caribou or any tracks.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>Sam Bosum says that we have to ask Simeon Mianscum, Tallyman for trapline W12.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trapline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Sam Bosum would like Barrette-Chapais Ltée to repair the road (#11 on the map) that goes to his main camp (#12 on the map) situated at Waposite Lake. This road would be inside the limits of the future Assinica Park.</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>Sam Bosum says there are none on the proposed layout.</p>
<p>Q5 : Is there an ancestral site in the corridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>Sam Bosum says there are none on the proposed layout.</p>
<p>Q6 : May we use your actual or traditional knowledge in the present study of this impact ?</p> <p>Sam Bosum shows his (#13 on the map) near Lake Ruth that his family uses for hunting geese in the spring, but says that the project of this road will not affect this activity</p> <p>Sam Bosum has never seen caribou or their tracks around Ruth Lake and he doubts the study made by the MRNF.</p>
<p>Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?</p>

Yes, you may show the family geese hunting grounds (#13 on the map).

Q6.2 : If not, why would you refuse to give us this information ?

N/A

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

N/A

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

Sam Bosum mentioned that there are hydrocarbon containers that were buried by Hydro-Québec during construction of the electric lines (#14 on the map).

Sam Bosum also mentioned that he would like construction and maintenance of the road be done with equipment from Oujebougoumou Community.

Sam Bosum also mentioned that Charlie Bosum has a loader and gravel truck.

Q9 : On the whole, do you agree with this project ?

Sam Bosum says he agrees and understands that this road will be safer.

Private Consultations with Tallymen who might be affected by the Construction of Forestry Road

Extension L-209-N

Report of this Consultation

Date : November 20, 2009
Place : Barrette-Chapais Ltée
Trapline Concerned : O53 Consultation simultaneously with trapline O52.
Person Concerned with this trapline : Charlie Bosum
Persons present during the private Consultation :
Charlie Bosum, tallyman for trapline O53
Walter Bosum, trapline O53 tallyman's son
Sam Bosum, trapline O52 tallyman's brother
Tommy RabbitSkin, JWG coordinator from Oujeboougoumou
Jean-Luc Bugnon, Environnement industriel Bugnon inc.
Denis Chiasson, Barrette-Chapais Ltée
Editor : Denis Chiasson
<h3>Presentation of the Project</h3> <p>A specific map of the Trapline affected by this project was presented. This map shows the proposed layout, as well as all Company information. Not only important points will be presented, but also any incomplete or missing information.</p>

Specific questions
<p>Q1 : What route seems to be better – the layout going west or east of Ruth Lake ?</p> <p>No comment from Charlie Bosum who seems to agree with comments from trpline tallyman O52.</p>
<p>Q1.1 : For what reason ?</p> <p>Idem.</p>
<p>Q2 : What measures do you think are possible that could reduce or improve expected impacts from activities on Lake Caupichigau ?</p> <p>Idem.</p>
<p>Q3 : What layout planning or infrastructures could make the project easier for this trpline ? For example : parking, portages, snowmobiles trails,...) ?</p> <p>Charlie Bosum asked that the road leading to his main camp (#1 on the map), on Lac à l'eau Noire, be finished by Barrette-Chapais Ltée, because this road was constructed but doesn't have gravel on it (#2 on the map).</p> <p>Charlie Bosum shows us his brother's camp (#3 on the map) it is near route L-209N about 3 km right after the beginning of the continuation of route L-209N which is under study. He tells us that the existing road is quite muddy during the spring (#4 on the map) and that plans should be made for this road to have easier access to his camp.</p>
<p>Q4 : Do you know of any spawning areas or species at risk that could be affected by this road project ?</p> <p>In the proposed layout, Charlie Bosum says that there are no spawning areas.</p> <p>However, he mentioned that Barrette-Chapais Ltée had once built a bridge on one spawning area, the species that he wants to keep confidential. He asked that the bridge be relocated. He also mentioned that this species is very much affected by trucks passing in this area, and that this area should not be used during the spawning season.</p>
<p>Q5 : Is there an ancestral site in the corridor of the proposed road ? If yes, would you please let us know where it is ?</p> <p>Charlie Bosum says there was a site of interest for moose (#5 on the map) before trees were harvested in this area, but now there is no more interest.</p>
<p>Q6 : May we use your actual or traditional knowledge in the present study of this impact ?</p> <p>Yes except for the spawning.</p>

Q6.1 : If yes, could you please show us on the map what are the points of interest that we could show ?

Yes, to indicate his brother' camp (#3 on the map).

Do not show spawning areas.

Yes, to indicate the location of the dangerous bridge (#6 on the map).

Q6.2 : If not, why would you refuse to give us this information ?

Concerning the spawning areas, Charlie Bosum doesn't want others to take advantage, thus affecting productivity.

Q7 : Without revealing any confidential points, could you tell us where the portage passages are, the temporary and permanent camps, snowmobile trails, burial grounds, etc... ?

N/A

Q8 : Are there any specific points that might have forgotten and that you wants to bring up ?

Charlie Bosum mentioned that the bridge on Brock River at km 46 on route L209N (#6 on the map) is not safe. He wants this bridge to be relocated at (#7 on the map)

Charlie's son Walter mentioned that the are hydrocarbon containers that were buried by Hydro-Quebec during construction of th electric lines (#8 on the map).

Q9 : On the whole, do you agree with this project ?

No comment from Charlie Bosum.

Appendix 10.1

Correspondence sent by the *MRNF – Direction de la Faune* regarding the Lake Thomelet
River

De : Mathieu.Morin@mrfn.gouv.qc.ca [mailto:Mathieu.Morin@mrfn.gouv.qc.ca]

Envoyé : 20 novembre, 2009 14:17

À : Marceau, Dario

Cc : Josee.Boivin@mrfn.gouv.qc.ca; Sylvie.Beaudet@mrfn.gouv.qc.ca

Objet : RE : carte

Importance : Haute

Salut Dario,

Voici mes commentaires. Désolé des délais, on devait éclaircir notre rôle dans le processus d'évaluation environnemental de votre projet d'abord. Si Sylvie a des commentaires à ajouter, elle te les fera parvenir à son retour. Pour toute question ou précision n'hésites pas.

Restreindre l'accès ne devrait pas être l'objectif recherché dans l'aménagement de la traverse de cours d'eau. Les objectifs de protection de la qualité de l'eau et de l'habitat doivent demeurer en tête de liste. N'oublions pas qu'il s'agit de lacs publics donc tout citoyen québécois a le droit d'y accéder et que la Loi sur la conservation et la mise en valeur de la faune interdit tout entrave à la réalisation d'activités de chasse, pêche ou piégeage (C-61.1, a.1.3 et 1.4). Il faudrait donc être très prudent si on désire aménager dans le but de repousser les pêcheurs. L'aménagement de nouveaux accès sur d'autres plans d'eau est une option de mise en valeur envisageable. On devra cibler des lacs de grande superficie comme le lac Omo situés à l'extérieur de la Réserve faunique Assinica ou du futur parc Assinica. Aussi, on devra s'assurer de l'entretien de ces infrastructures durant leur durée de vie utile afin d'éviter à long terme toute modification de l'habitat de poisson. Il est évident que cette situation générera un attrait pour le développement de la villégiature privée ce qui ajoutera une pression de pêche supplémentaire à celle des pêcheurs journaliers et des pêcheurs de subsistance qui le fréquentent ou le fréquenteront. D'où l'importance de cibler des lacs de grande superficie qui permettent un plus grand effort de pêche. Si des problèmes évidents de baisse de la qualité de pêche seraient observés sur un lac, le MRNF pourrait intervenir avec des mesures réglementaire pour réduire la pression de pêche mais certainement pas sur la base de demandes personnelles d'utilisateurs. En même temps, pour répondre à une possible baisse de qualité de pêche, on doit faire un suivi, ce qu'on ne fait pas en territoire libre. D'où l'importance d'être prudent quand on vise à mettre en valeur des lacs spécifiques.

Ceci dit, mon avis est que la variante contournant le lac Ruth à l'est permettrait d'éviter ces préoccupations ou du moins de les amoindrir en plus d'éviter de traverser un cours d'eau majeur (selon ta carte). Il y a sans doute d'autres intrants à considérer dans le choix de la variante mais comme nous n'avons pas l'étude d'impact en main et que le MDDEP ne nous sollicite pas sur la globalité du projet, on ne peut s'avancer davantage.

C-61.1 LOI SUR LA CONSERVATION ET LA MISE EN VALEUR DE LA FAUNE

CHAPITRE I.1

DROIT DE CHASSER, DE PÊCHER ET DE PIÉGER

2002, c.82, a.3 (eff. 2002-12-19).

Droits. — Restriction.

1.3. Toute personne a le droit de chasser, de pêcher et de piéger, conformément à la loi.

Le premier alinéa n'a pas pour effet, toutefois, d'établir une prépondérance de ce droit à l'égard d'autres activités pouvant s'exercer sur le même territoire.

2002, c.82, a.3 (eff. 2002-12-19).

Obstacle. — Interprétation.

1.4. Nul ne peut sciemment faire obstacle à une personne effectuant légalement une activité visée au premier alinéa de l'article 1.3, y compris une activité préparatoire à celle-ci.

Pour l'application du premier alinéa, on entend par «faire obstacle» notamment le fait d'empêcher l'accès d'un chasseur, d'un pêcheur ou d'un piégeur sur les lieux de chasse, de pêche ou de piégeage auxquels il a légalement accès, d'endommager le mirador ou la cache d'un chasseur, d'incommoder ou d'effaroucher un animal ou un poisson, par une présence humaine, animale ou toute autre, par un bruit ou une odeur ou le fait de rendre inefficace un appât, un leurre, un agrès, un piège ou un engin destiné à chasser, à pêcher ou à piéger cet animal ou ce poisson.

2002, c.82, a.3 (eff. 2002-12-19).

Mathieu Morin, biologiste, M.Sc.
Direction de l'expertise Énergie-Faune-
Forêts-Mines-Territoire du Nord-du-Québec
Ministère des Ressources naturelles
et de la Faune
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Chibougamau (Québec) G8P 2Z3
Tél: (418) 748-7701 poste 234
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Mathieu.Morin@mrfn.gouv.qc.ca

P Devez-vous vraiment imprimer ce courriel? Si oui, pensez l'imprimer recto-verso!

-----Message d'origine-----

De : Marceau, Dario [mailto:Dario.Marceau@ca.ebarrette.com]

Envoyé : 6 novembre 2009 11:59

À : Morin, Mathieu (10-DEX)

Objet : carte

Bonjour Mathieu,

Tel que convenu, voici les questions que le MDDEP nous posent concernant les 2 options que nous avons proposé dans notre étude d'impact. J'aimerais si possible recevoir une réponse d'ici le 13 novembre de votre part. J'ai joins une carte localisant les options.

Bonne journéé.

Dario M.

Question 6 : Quelles mesures d'atténuation pourraient être envisagées afin de restreindre l'accès au lac Caupichigau aux pêcheurs sportifs ? Entre autres, serait-il possible d'aménager les approches du pont ou les bords de la rivière de la traversée #10B de façon à ce qu'il soit impossible à cet endroit d'y mettre une embarcation à moteur à l'eau ? Et, serait-il possible d'aménager une rampe de mise à l'eau pour embarcations à moteur sur 1 ou 2 autres plans d'eau de ce secteur afin de détourner l'intérêt des pêcheurs ?

Question 7 : Compte tenu que la pêche sportive relève de la compétence du MRNF, le promoteur devra consulter ce Ministère afin de déterminer l'intérêt ou ce qui pourrait être entrepris afin de restreindre les activités de la pêche sportive au lac Caupichigau.

Appendix 10.8

Reports on the preliminary public consultations held and mentioned in the initial impact assessment documents



**Report on the public consultation held with the Oujé-Bougoumou community on June 17,
2009**

Prepared by: Denis Chiasson, forestry technician, BCL
and Jean-Luc Bugnon, EIB

Meeting location: Hôtel Harricana, Chibougamau

Start of meeting: 13:30

Subject: Impact assessment – L-209 North logging road

Mr. Jean-Luc Bugnon of the consulting firm Environnement industriel Bugnon Inc. started by welcoming all those in attendance at the information meeting. A dozen people were present, including Oujebougoumou Chief Mrs. Louise Wapachee, the members of the band council, the Director of Traditional Pursuits (Mr. Norman A. Wapachee), and the head of the Oujebougoumou Joint Working Group (Mr. Tommy RabbitSkin). However, the tallymen affected by the project were absent.

Mr. Bugnon explained to the audience the objective of the public consultation regarding the planned construction of the L-209 North logging road, included in Barrette-Chapais Itée's Forest Management Plan. Quebec's Environment Quality Act stated that any road longer than 25 km and with a useful life greater than 15 years is subject to an environmental impact assessment. This applies to this road since its length is 97 km and its useful life is greater than 15 years.

Mr. Bugnon gave a description of the mandate with respect to the environmental impact assessment undertaken by Barrette-Chapais: project rationale, description of environmental and social impacts, and consultation process. He also cautioned the audience that such a public consultation is intended neither to put the forestry industry on trial nor to debate forest management plans, but focuses rather on the construction of a road.

Mr. Bugnon then made a PowerPoint presentation detailing the scope of the ongoing impact assessment:

- Rationale for the construction of the road: First, guarantee Barrette-Chapais's sawmill timber supply; second, open up the territory to harvesting in a mosaic pattern, based on the distribution of "harvest with regeneration protection" (HARP) areas and the preservation of the habitats of the majority of wildlife species found on the territory.

Mr. Bosum asked if the planned road was of the same type as the current Oujebougoumou road. Mr. Bugnon answered yes.

- Description of the road to be built: The initial road measures in total 96 km; the alternate corridor measures 97 km; the road starts at the Chibougamau River interchange and ends in the northwestern part of Forest Management Unit (FMU) #026-65. The initial corridor includes three bridges and the alternate corridor, two.
- The alternate corridor was drawn as a response to the concerns surrounding access to Lake Caupichigau raised by Mr. Simeon Mianscum, the area #W12 tallyman. He opposes the passage of the road in this area because he believes this will increase access to Lake Caupichigau and traffic on the lake.
- This road is designed for the transportation of timber using oversized trucks; its right-of-way is 35 m wide; the roadway surface measures 10 m wide; the maximum safe speed is 70 km/hr; traffic frequency is 150 trucks per week, 24 hours a day on business days, over a period of approximately 6 weeks per year. A temporary camp will be needed during the construction of the road, near Lake Omo.

Mr. Wapachee asked how many people would use this camp and where would it be located. Mr. Bugnon answered that the camp was intended for 40 people and that it would be located near Lake Omo, on the same site as the camp set up by Hydro-Québec at the time of the construction of the power transmission line.

- Mr. Bugnon presented the land use ratios for forests, peat bogs, lakes and power transmission lines in each of the two assessment areas. He started with the overall assessment area, comprising all the trapping areas affected by the road's construction, and then discussed the local assessment area (500m on either side of the road).
- Woodland caribou are present in the local assessment area for the planned road; it is a threatened species. The caribou visit three parts of the overall assessment area.

The councillor at large of the band council asked if inquiries into the precise locations of the areas visited by the caribou had been addressed to the wildlife section of the *Ministère des Ressources naturelles*. Mr. Bugnon answered that such inquiries had indeed been made and that the MRNF had confirmed the presence of woodland caribou in those areas, but had refused to divulge their exact location in order to protect this species.

Mr. Bosum identified several areas visited by woodland caribou.

- Mitigation measures will be developed in the course of impact assessment. During road construction operations, it will be necessary to protect water quality during the installation of bridges and culverts; woodland caribou might be disturbed by construction activities. When the road becomes operational, caribou might be disturbed by timber transportation activities carried out in the fall.

Mr. Wapachee stated that two Oujebougoumou tallymen were affected by the alternate corridor; he asked whether we had met the tallymen and whether we had used the information from the "family map". Mr. Bugnon mentioned that the tallymen had been consulted regarding the General Forest Management Plans and that we had relied on available information, i.e. the map of sites of special interest (1%) and the map of sites of special faunal interest (25%) but that we were unable to submit them with the environmental impact assessment since this information was confidential. However, the assessment includes statistics regarding the footprint of the road in such sites of special interest. The topic would be revisited later in the presentation.

A member of the audience asked what distance would separate the road from the Assinica Park project. He also stated that this road would have impacts on the habits of caribou due to the disturbances associated with the road, adding that the road would affect the caribou

over a longer period than the useful life of the road. Mr. Bugnon indicated on the map the location of the road relative to the Assinica Park project. He added that he did not know whether the impacts would be permanent, but that there indeed would be impacts.

Mr. Bosum stated that this road would have a significant impact since it would create a new access road for mining and forestry companies, as well as Hydro-Québec and traffic would increase. The road would also open up the territory near the future Assinica Park to hunters, fishermen and vacationers.

Mr. Bugnon mentioned that Waswanipi wanted the road to remain open all year long, even though Barrette-Chapais plans to use it 6-8 months per year.

Mr. Wapachee stated that the road was very close to the sensitive caribou zone.

Mr. Bugnon stated that the MDDEP had requested that the road avoid the Park and follow the old Hydro-Québec road as much as possible, given that the impact on the caribou would be significant and permanent. Mr. Bugnon added that following the existing road would have a positive impact versus using another corridor. He also mentioned that Barrette-Chapais wished to obtain the authorization to start road construction work this year.

Mr. Bugnon showed the overlap ratios (%) in the assessment area, the corridor and the local assessment area (500m) for each trapping area, site of special interest and site of special faunal interest crossed by the road.

Another member of the audience asked if there would be another meeting once the environmental impact assessment was completed. Mr. Bugnon answered that the study would be submitted at the end of June and that a meeting with each tallyman was desirable in order to define precisely the location of the road corridor within the local assessment area (500m).

Mrs. Louise Wapachee asked Mr. Norman A. Wapachee to organize a meeting with the tallymen to keep them informed and get their input regarding this road.

Mr. Bugnon asked Mrs. Wapachee whether the Oujeboougoumou community was interested by the employment opportunities related to this logging road project.

Mrs. Wapachee stated that the community was very interested.

At the end of the presentation, Mr. Bugnon reiterated the need to obtain feedback from the community regarding this project during the environmental impact assessment and invited all those interested to share their views on the project.

Mr. Norman A. Wapachee said that he would send us the tallymen's comments. He also added that he was very glad that we had come to consult the Oujeboougoumou community and make this presentation and that he felt this reflected very well on the project.

The meeting ended at 14:30.

Prepared by: Denis Chiasson, forestry technician
Barrette-Chapais Itée



**Report on the public consultation held with the Waswanipi community
on June 15, 2009**

Prepared by: Denis Chiasson, forestry technician, BCL
and Jean-Luc Bugnon, EIB

Meeting location: Youth Center of Waswanipi

Several people came to inquire about the proposed road corridor and to ask questions before the beginning of the presentation. This road appears to stir a high level of interest within this community.

Start of meeting: 13:00

Subject: Impact assessment – L-209 North logging road

Mr. Jean-Luc Bugnon of the consulting firm *Environnement industriel Bugnon Inc* welcomed all those present for the information meeting. About twenty people were in attendance, including the Waswanipi Chief. The meeting was simultaneously interpreted in Cree.

Mr. Bugnon explained to the audience the objective of the public consultation regarding the planned construction of the L-209 North logging road, included in Barrette-Chapais Itée's Forest Management Plan. Quebec's Environment Quality Act states that any road longer than 25 km and with a useful life greater than 15 years is subject to an environmental impact assessment. This applies to this road since its length is 97 km and its useful life is greater than 15 years.

Mr. Bugnon gave a description of the mandate with respect to the environmental impact assessment undertaken by Barrette-Chapais: project rationale, description of environmental and social impacts, and consultation process. He also cautioned the audience that such a public consultation is intended neither to put the forestry industry on trial nor to debate forest management plans, but focuses rather on the construction of a road.

Mr. Bugnon then made a PowerPoint presentation detailing the scope of the ongoing impact assessment:

- Rationale for the construction of the road: First, guarantee a supply of timber for Barrette-Chapais Itée's sawmill; second, open up the territory to harvesting in a mosaic pattern, based on the distribution of "harvest with regeneration protection" (HARP) areas and the preservation of the habitats of the majority of wildlife species found on the territory.

- Description of the road to be built: The initial road measures in total 96 km; the alternate corridor measures 97 km; the road starts at the Chibougamau River interchange and ends in the northwestern part of Forest Management Unit (FMU) #026-65. The initial corridor includes three bridges and the alternate corridor, two.
- The alternate corridor was drawn in response to the concerns surrounding access to Lake Caupichigau raised by Mr. Simeon Mianscum, the area #W12 tallyman.
- This road is designed for the transportation of timber using oversized trucks; its right-of-way is 35 m wide; the roadway surface measures 10 m wide; the maximum safe speed is 70 km/hr; traffic frequency is 150 trucks per week, 24 hours a day on business days, over a period of approximately 6 weeks per year. A temporary camp will be needed during the construction of the road, near Lake Omo.
- Mr. Bugnon presented the land use ratios for forests, peat bogs, lakes and power transmission lines in each of the two assessment areas. He started with the overall assessment area, comprising all the trapping areas affected by the road's construction, and then discussed the local assessment area (500m on either side of the road).
- Woodland caribou are present in the local assessment area for the planned road. It is a threatened species. Caribou visits three parts of the overall assessment area.
- Mitigation measures will be developed in the course of the impact assessment. During the road construction operations, it will be necessary to protect water quality during the installation of bridges and culverts. Woodland caribou might be disturbed by construction activities. When the road becomes operational, caribou might be disturbed by timber transportation activities carried out in the fall.
- Barrette-Chapais will implement an environmental impact monitoring and follow-up program in accordance with the ISO 14001 standard with respect to the road construction project.

At the end of the presentation, Mr. Bugnon reiterated the need to obtain feedback from the community regarding this project. Members of the audience were invited to share their comments regarding the following issues:

- Can you identify areas that are at risk?

Mr. Simeon Mianscum, a tallyman affected by the project, expressed deep concern regarding the impact of this project on his trapping area #W12; Caupichigau. He explained, very emotionally, that this would attract a large number of people, in particular fishermen who would have access to Lake Caupichigau once the road was built. To sum up, he was completely opposed to the project.

Mr. Allan Saganash, local coordinator of the MRNF-CREE Joint Working Group, asked us to clarify whether the assessment would only cover a 35m wide area. Mr. Bugnon clarified that the assessment would cover an area 500m on either side of the road, in addition to the overall assessment area.

Mr. Neeposh, son of a tallyman affected by the project (trapping area #W10), asked whether it would be possible to close the road temporarily during the two week Canada goose hunting season in spring and during the two week moose hunting season in fall. Mr. Bugnon replied that it would not be possible to close the road. Mr. Neeposh then requested that Barrette-Chapais at least cease operations during these periods; he stated that it was a security issue.

Mr. Neeposh also indicated on the displayed map a road that he wanted to see improved, as well as a water spring that should be protected. He asked that the road avoid coming too close to lakes in order to make them less accessible to fishermen.

Mr. Simeon Mianscum addressed the meeting again, and requested that work be suspended on weekends, i.e. from Thursday night to Sunday.

Mr. Allan Saganash asked whether Barrette-Chapais would hire contractors from the Waswanipi community. He expressed the wish that members of his community be given the opportunity to apply for jobs. He requested that each tallyman affected by the project be consulted and given the opportunity to express their views once the impact assessment was completed.

Mr. Albert Trapper (son of the tallyman for area #W10A, affected by the project) wanted to know the location of the temporary camp. Mr. Bugnon showed him the planned location of the camp on the map. Mr. Trapper also stated that old batteries (the size of transformers) had been buried by Hydro-Québec during the construction, 30 years ago, of the road under the power transmission line.

He was surprised to learn that a camp was planned at this location, since this fact had not been mentioned during the development of the General Forest Management Plans for this territory, a process in which he participated. Mr. Bugnon informed him that this camp was temporary. It would only be used during construction of the road. Mr. Trapper asked how we planned to dispose of waste. Mr. Bugnon answered that domestic waste would be placed in a trench. Mr. Trapper informed us that previously planned roads close to the planned corridor had all been rejected by the MRNF. Denis Chiasson of Barrette-Chapais confirmed this information, but added that these roads had been proposed in the context of a project to restart production, following the fire in 1986. Since the MRNF never responded to the corresponding application for financing, the project never took place.

A member of the audience, who uses one of Barrette's timber supply territories further to the south, explained that he had already asked Barrette to repair the road surface of a bridge that had become damaged due to weather and where his tires regularly punctured. Denis Chiasson informed him that a request to repair this bridge had been included in the 2009-10 Annual Forest Management Plan.

Mr. Neeposh requested that no prescribed burning be carried out in the fall.

Mr. Bugnon asked the following question: Will this road help you to reach your trapping area?

Mr. Neeposh answered no, since a road already exists.

Mr. Paul Dixon, local coordinator of the Waswanipi Cree Trappers' Association, expressed concern over the fact that this road would attract tourists, fishermen, hunters and vacationers. He too expressed the wish that waste be taken back where it came from rather than buried in trenches.

A member of the audience mentioned that several culverts located under the old Hydro-Québec power line road were washed out; she asked who would be responsible for maintaining the road once it was completed: the Waswanipi band council or Barrette-Chapais? Mr. Bugnon told her that the road would be regularly used during a period of at least 15 years and that Barrette-Chapais Itée would be responsible for its maintenance.

Mr. Mianscum asked what the planned road construction work schedule was. Mr. Bugnon informed him that work would take place from Monday to Friday.

Mr. Trapper asked whether the road would be open all winter. Mr. Bugnon indicated that the road would be used to transport timber and that it would be open during transportation periods. Mr. Trapper stated that companies were not concerned about trappers.

Mr. Bugnon explained that we had an obligation to take note of comments and that the MDDEP would make any recommendations that were necessary. The road construction project would only be approved subject to following all such recommendations.

Mr. Dixon stated that if the road were kept open 365 days per year, he would see the project in a positive light.

Mr. Saganash stated that the access routes to trapping areas created by this project would increase the presence of several people. He also stated that Waswanipi had already been considerably affected by the forestry and mining industries.

A member of the audience stated that several non-native contractors were in the habit of breaking beaver dams during road construction and that he wished that trappers be invited to help during this project. He also mentioned that a large amount of dust was raised near Aboriginal camps. Mr. Bugnon informed him that it was possible to use lignosulfates.

Mr. Neeposh stated that the large trucks used for transportation were dangerous. He suggested that interactive signs be used to indicate the periods during transportation took place.

Another member of the audience asked that parking spaces and boat launching ramps be set up at each bridge, subject to the agreement of trappers.

Mr. Derrick Neeposh requested that warning signs be installed near camps in order to slow traffic. He also wished to know whether another company, such as the Waswanipi Mishtuk Corporation, would be able to use the road.

Denis Chiasson of Barrette answered that this road would be public and that any other user could use it.

Mr. Paul Dixon stated that, in his opinion, culverts were in general too small.

At the end of the question period, Mr. Bugnon invited those who wished to do so to submit their comments in writing, by mail or by email.

The meeting ended at 17:15.

Prepared by: Denis Chiasson, forestry technician
Barrette-Chapais Ltée



**Report on the public consultation held with the population of Chapais et Chibougamau
on April 27, 2009**

Prepared by: Denis Chiasson, forestry technician, BCL
and Jean-Luc Bugnon, EIB

Meeting location: Chapais City hall

Start of meeting: 19:00

Subject: Impact assessment – L-209 North logging road

Mr. Jean-Luc Bugnon of the consulting firm Environnement industriel Bugnon Inc. welcomed all those that had come to the information meeting. About twenty people were in attendance, including the mayor of Chapais.

Mr. Bugnon explained to the audience the objective of the public consultation regarding the planned construction of the L-209 North logging road, included in Barrette-Chapais Itée's Forest Management Plan. Quebec's Environment Quality Act states that any road longer than 25 km and with a useful life greater than 15 years is subject to an environmental impact assessment. This applies to this road since it is 100 km long and has a useful life greater than 15 years.

Mr. Bugnon gave a description of his mandate with respect to the environmental impact assessment undertaken by Barrette-Chapais: project rationale, description of environmental and social impacts, and consultation process. He also cautioned the audience that such a public consultation is intended neither to put the forestry industry on trial nor to debate forest management plans, but focuses rather on the construction of a road.

Mr. Bugnon then made a PowerPoint presentation detailing the scope of the ongoing impact assessment:

- Rationale for the construction of the road: First, ensure Barrette-Chapais Itée's sawmill is supplied in timber; second, open up the territory to harvesting according to a mosaic pattern, based on the distribution of "harvest with regeneration protection" (HARP) areas and the preservation of the habitats of the majority of wildlife species found on the territory.

(Ref.: MRNF.GOUV.QC.CA : Mosaic cutting is a harvesting strategy practiced in forests that are dominated by softwood species and where trees are mature and have approximately the same age. It consists in distributing HARP areas of varying shapes and sizes on a given territory in order to produce, in the medium term, a mosaic of stands of varying age categories. In this way, the natural diversity of the forest is preserved, thus fulfilling the needs of the various wildlife species that inhabit it.)

- Description of the road to be built: The initial road measures in total 100 km; the alternate corridor measures 97 km; the road starts at the Chibougamau River

interchange and ends in the northwestern part of Forest Management Unit (FMU) #026-65.

- This road is designed for the transportation of timber using oversized trucks; its right-of-way is 35 m wide; the roadway surface measures 10 m wide; the maximum safe speed is 70 km/h; traffic frequency is 150 trucks per week, 24 hours a day on business days, over a period of approximately 6 weeks per year.
- Description of the natural and social environment: the road is to be located within the boreal forest, where woodland caribou is found. An archaeological site has been listed near Lake Capiechigamau; the territory also contains hunting and Aboriginal camps.
- Impacts associated with the construction and operation stage of the project are not different from those of other roads.
- Mitigation measures will need to be developed in the course of the environmental impact assessment, in particular with respect to the woodland caribou.
- Barrette-Chapais will implement an environmental impact monitoring and follow-up program in accordance with the ISO 14001 standard with respect to the road construction project.

At the end of the presentation, Mr. Bugnon invited members of the audience to express their opinions regarding the project and asked a series of question aimed at eliciting their viewpoints. They were free to identify themselves or not. Only opinions and questions regarding the construction of the road would be noted in the environmental impact assessment.

- Do you use this territory?

Answer: Mr. Marc Bouchard, prospector and fisherman: yes, he uses this territory, via the old road along Hydro-Québec's power transmission line. He asked when the road would be built.

Answer: Mr. Bugnon: construction of the road requires ministerial approval. The date at which the work begins is therefore outside Barrette-Chapais Itée's control.

Mr. Bouchard asked whether the corridor that had been presented was similar to a tree trunk. Yes, given that the "branches" are not covered by the environmental impact assessment.

Mr. Bouchard asked over how many years would the harvest spread? Answer: Mr. Bugnon: Harvesting would take place on approximately 1% of the productive forest area annually. Hence, the harvest will be spread over several years.

Mr. Denis Meunier: Is harvesting planned in the area at the very end of the road or along its entire length?

Answer: Mr. Bugnon: Mainly in the northwestern part of the territory.

Anonymous question: Why not use the existing road?

Answer: Mr. Bugnon: For safety reasons.

Anonymous question: The existing road passes near power lines and towers. The new road will improve access to the area for the purposes of hunting and fishing.

In answer to that comment, Mr. Bugnon underscored that the northwestern part of the territory lies on Category II lands, where hunting and fishing are the exclusive right of the Aboriginal population.

Anonymous comment: In addition to better access for fishing, the project is good for the region's economy, since this area has a lot of potential for mining exploration.

Mr. Marc Bouchard asked in which area the woodland caribou could be found? Answer: Mr. Bugnon showed the approximate location of this area on the map on display.

Anonymous question: What measures are planned to protect the caribou?

Answer: Mr. Bugnon stated that the MRNF has an action plan and that Barrette-Chapais will base itself on this action plan.

Anonymous comment: This road could be used to transport ore.

Furthermore, in addition to giving access to the area for the purpose of fishing and hunting, this road will be safer and ultimately help keep jobs in the region.

Anonymous comment: This road would attract more tourists from Montreal and other places who are interested in fishing or hunting, which is a positive.

Mr. Marc Bouchard: Are First Nations in favour of the project? Answer: Mr. Bugnon stated that discussions had been launched during meetings held for the purpose of preparing the General Forest Management Plan, and that public consultations would also be held with the communities in order to elicit their viewpoints.

Anonymous questions: I would like to know what exactly mosaic cutting is. How long will the timber reserves last? Answer: Mr. Bugnon: Mosaic cutting is a technique that spreads the harvest in order to limit disturbances to the natural environment. Mr. Michel Deshaies of Barrette-Chapais added that forest resources, contrary to mineral resources, are renewable. He explained that each Forest Management Unit (FMU) is surrounded by trapping areas, on which it is mandatory to maintain at least 30% of the forest cover taller than 7 m at all times; in this way, it is possible to modulate harvesting operations and ensure the forest's renewal.

Anonymous: How long will the road's construction take? Answer: Mr. Deshaies: Probably around 1 or 2 years.

Anonymous questions: Will there be a call for tenders regarding the road's construction? Answer: Mr. Deshaies: The road will be built using internal resources.

Representative of the chamber of commerce: I came here to get information regarding the project. She also mentioned that she supports the project.

Mayor of Chapais: The city of Chapais is in agreement with the planned construction of the road; this project will ensure Barrette-Chapais Ltée's sawmill has a supply of timber; this project is good for employment in the region of Chibougamau-Chapais, as well as for the mining and tourism industries. The road will be safer and it will contribute the region's development.

Comment from a person who works in forest machinery maintenance: Truckers who have transported timber in this area have told me that the road is dangerous because it passes near Hydro-Québec power lines; I came here tonight to learn more about this project.

Comment: The current road has degraded a lot in the last 25 years. Improving it would give a boost to its users; it would also make mineral exploration easier.

Mr. Marc Bouchard: I support this project.

Mr. Denis Meunier: I am also in favour of it.

Mr. Mayor: I also support this project and, in particular, the development of BCL.
The mayor also asked whether other consultations would be held.

Mr. Bugnon: No, except in the Cree communities.

Mr. Bugnon invited all to communicate their comments in writing if they so desired.

The meeting ended at 20:00.

Prepared by: Denis Chiasson, forestry technician
Barrette-Chapais Itée